

## Great South Wall Overview of Impacts, Mitigation & Interpretation





## Contents

**This document is to be read in conjunction with the following drawings and documentation:**

RPS Engineering Drawings (S26-HER-XX-DR-HE 100-1000 to 1006)

Darmody Architecture Active Travel Drawings CP1901\_011-DA-00-XX-DR-A-(PA-003-PA-005) & associated detail drawings

Chapter 16 of the EIAR: Cultural Heritage (Including Archaeological, Architectural & Industrial) , prepared by ADCO / RPS

Appendices to Chapter 16, in particular Appendix 16-6 - Specifications and Method Statement for Conservation and restoration of the Great South Wall parapets

Dublin Port Heritage Conservation Strategy, prepared by RPS, ADCO, Southgate Associates, MOLA Architecture, and Shaffrey Architects

Darmody Architecture Active Travel Drawings CP1901\_011-DA-00-XX-DR-A-(PA-003-PA-005) & associated detail drawings

Active Travel Landscape Architecture Drawings & Landscape Design Report, prepared by: TTT - (thirtythreetrees)

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## Section 01 - Introduction & Context

### 3FM Project - Great South Wall Overview of Impacts, Mitigation & Interpretation



▲ Painting: A View of the Pigeon House, Dublin by William Sadler, 1810  
Source: National Gallery of Ireland

#### Introduction to Document

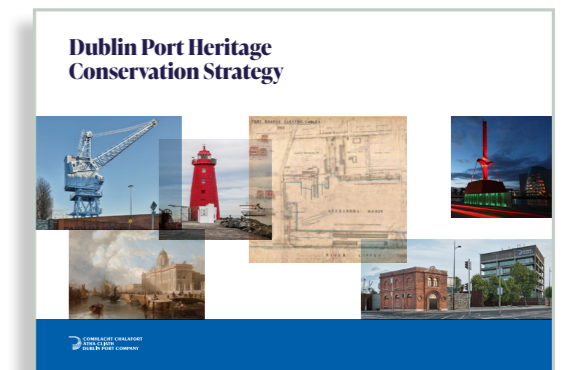
The Great South Wall stands as a testament to historical engineering and maritime heritage, yet its presence and significance have gradually faded amidst the ever-evolving urban landscape. Portions of the wall are currently buried under new infrastructures, while other sections remain above ground, with some parts more intact than others. The necessity to preserve and highlight this important historical structure has never been more urgent. This document outlines a plan to restore the visibility and prominence of the Great South Wall, ensuring its legacy is preserved for years to come.

This endeavor is the result of a collaborative effort involving archaeologists, conservation

experts, and engineers working on new road proposals. Their combined expertise has been instrumental in shaping a project that not only respects the historical and cultural significance of the wall but also integrates modern infrastructural needs. This document serves to consolidate these multidisciplinary contributions into a cohesive interpretative proposal.

By implementing strategic measures to enhance the wall's legibility, the project aims to educate the public about its historical importance and improve the overall visitor experience. These efforts will help foster a renewed connection to the area's rich heritage, creating vibrant, engaging environments that celebrate its past.

Dublin Port Heritage Conservation Strategy and its authors / contributors





The Great South Wall

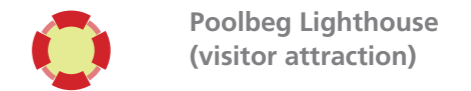


**The Great South Wall**

The Great South Wall (GSW) of Dublin Port is a significant feat of engineering and maritime construction from the 18th century. It was originally 3 miles long (stretching at nearly 5km) and was one of the longest sea walls in the world at the time of its completion.

The construction of the Great South Wall began in the early 18th century, with the primary purpose of improving the navigability and safety of Dublin Port. The wall was designed to protect the port's entrance from the accumulation of sandbanks and to provide a safer passage for ships.

The wall includes the iconic Poolbeg Lighthouse at its eastern end, completed in 1768, which became a critical navigational aid for ships entering Dublin Bay. The lighthouse was originally lit by candlepower and later converted to oil, and it remains a distinctive landmark.



◀ Aerial view of Dublin Port lands and the stretch of the Great South Wall located from where it begins to where it ends with the Poolbeg Lighthouse





## Introduction to 3FM Project



The 3FM Project is the third and final Masterplan project needed to bring Dublin Port to its ultimate capacity by 2040. The 3FM Project is a key part of Dublin Port Company's commitment under Masterplan 2040 to provide additional capacity for future growth by maximising the use of existing port lands. The proposed development focuses on Dublin Port Company-owned lands on the Poolbeg Peninsula, where one-fifth of the Dublin Port estate is located. This is also known as the south port area.

### Rationale for the 3FM Project:

#### 1. Ultimate Port Capacity

- The Dublin Port Masterplan 2040, reviewed 2018, determined that the port's ultimate capacity was 77.2m tonnes of cargo throughput per annum by 2040 based on the brownfield land available to the port. Since then, however, there has been a permanent loss of 7ha of port land to State Services in the North Port, primarily for the Office of the Revenue Commissioners, Customs Division as a result of Brexit. The consequence of this loss of land has been to reduce the port's

ultimate capacity to 73.8m tonnes of cargo throughput per annum by 2040.

#### 2. Terminal Capacities

- A new Lift-on Lift-off (Lo-Lo) container terminal with an annual throughput capacity of 550,000 Twenty-foot Equivalent Units (TEU) or 5.34m tonnes.

The Lo-Lo container terminal will consist of two main components:

- Terminal located north of the ESB's Generating Station on the eastern end of Poolbeg Peninsula with 650m of deep water berthage dredged to a depth of -13.0m CD (Chart Datum), plus associated cargo handling areas (Dublin Port Masterplan Area N). This terminal will accommodate larger Lo-Lo vessels of up to 240m length, primarily from Continental Europe.
- Transit container storage yard located on waterside land currently used for bulk cargo handling (Dublin Port Masterplan Area L).

- Replacement of the existing Lo-Lo container terminal, currently operated by Marine Terminals Limited (MTL), with a new Roll-On Roll-Off (Ro-Ro) freight terminal with an annual throughput capacity of 360,000 Ro-Ro units or 8.69m tonnes.

The Ro-Ro freight terminal will consist of two main components:

- Terminal located at existing Berths 42 – 45 including provision of two berths, each with a single tier Ro-Ro ramp, plus associated cargo handling facilities (Dublin Port Masterplan Area K).
- Terminal located on Port owned land on the southern side of the Poolbeg Peninsula (Dublin Port Masterplan Area O).

This combined terminal will accommodate larger Ro-Ro vessels of up to 240m length, primarily from Continental Europe.



- 3FM 'Community Gain' proposals within 3FM Project includes the subject Port Park, Active Travel Route, & Maritime Village

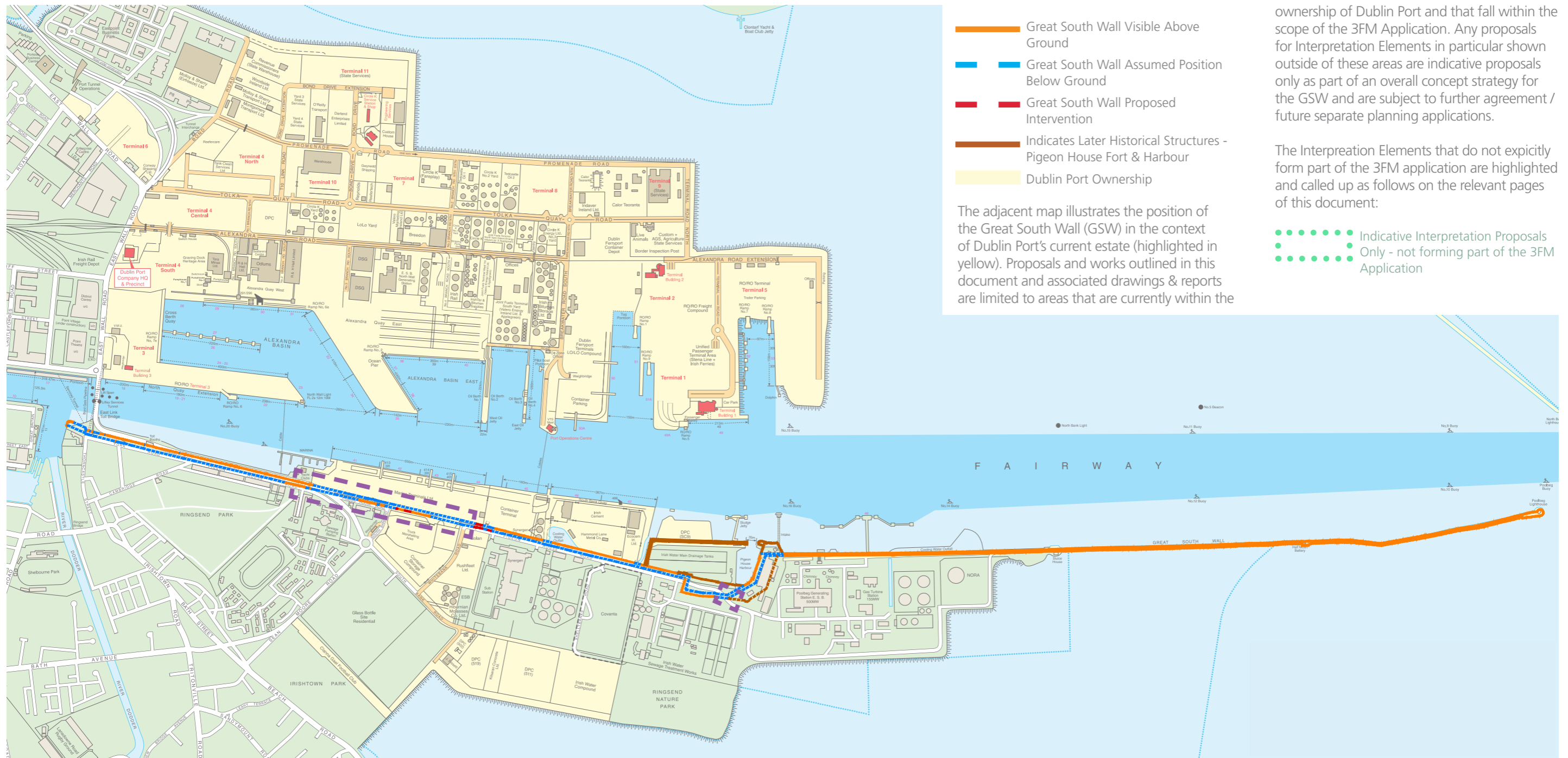


- Extract from DPC Masterplan 2040 - indication of inland and Portside lands covered in the Dublin Port Estate, used for shipping, cargo handling and storage, ferry and cruise ship activities, and leisure boating areas

- Aerial view of Dublin Port north & south lands subject to Third and Final Masterplan Project



# Dublin Port Ownership - Constraints & Limitations



- Great South Wall Visible Above Ground
- Great South Wall Assumed Position Below Ground
- Great South Wall Proposed Intervention
- Indicates Later Historical Structures - Pigeon House Fort & Harbour
- Dublin Port Ownership

ownership of Dublin Port and that fall within the scope of the 3FM Application. Any proposals for Interpretation Elements in particular shown outside of these areas are indicative proposals only as part of an overall concept strategy for the GSW and are subject to further agreement / future separate planning applications.

The Interpretation Elements that do not explicitly form part of the 3FM application are highlighted and called up as follows on the relevant pages of this document:

- Indicative Interpretation Proposals Only - not forming part of the 3FM Application

The adjacent map illustrates the position of the Great South Wall (GSW) in the context of Dublin Port's current estate (highlighted in yellow). Proposals and works outlined in this document and associated drawings & reports are limited to areas that are currently within the

▲ Map of Current Dublin Port Ownership with route of Great South Wall highlighted





## History of the Great South Wall



▲ Historic reconstruction drawing showing the East Wall in early 1700s by Uto Hogerzeil  
Source: Uto Hogerzeil for DCP



▲ A survey of the city of Dublin, John Roque, 1757  
Source: Bibliotheque Nationale De France

▼ Pigeon House at the time of the Act of Union c1801  
Source: Archiseek



▲ Painting of Poolbeg Lighthouse c1820  
Source: Dublin Port Archive



**The construction of the Great South Wall (GSW) was a monumental engineering project aimed at controlling the movement of sands and facilitating easier navigation in Dublin Bay.**

The initial effort was focused on shaping the river channel to the east of Ringsend, where sands from the South Bull and Sandymount area were moving northwards over the delta. In 1716/17, timber piling operations were initiated to establish a breakwater between the Green Patch, a natural shallows to the east of Ringsend, and Poolbeg Light. By 1731, the breakwater, known as 'The Piles', was completed, spanning 3,109m in length.

In 1748, the Ballast Board proposed the construction of a double stone wall that would extend 2,100m from the western end of the Piles to Ringsend Point. The wall, completed in 1759, was named the Ballast Office Wall and consisted of two parallel rubble walls with sand filling the 37 to 48 feet wide space in between. Today, this stretch of the wall lies beneath Pigeon House Road, but some sections of its boundary walls are visible as low parapet walls, and a portion of the north-facing wall is exposed to its full height along the cooling water outfall of the ESB Dublin Bay Power Plant.

Construction of an extension to the wall towards Poolbeg began in 1761, taking over 30 years to complete. The extension was built westwards from Poolbeg and on the north side of the Piles. Early works included laying the foundations for Poolbeg Lighthouse in 1764, which was completed in 1767. The main wall length has a different construction from the Ballast Office Wall, featuring a dry rubble core with granite ashlar in distinctive linear-shaped blocks forming the two façades

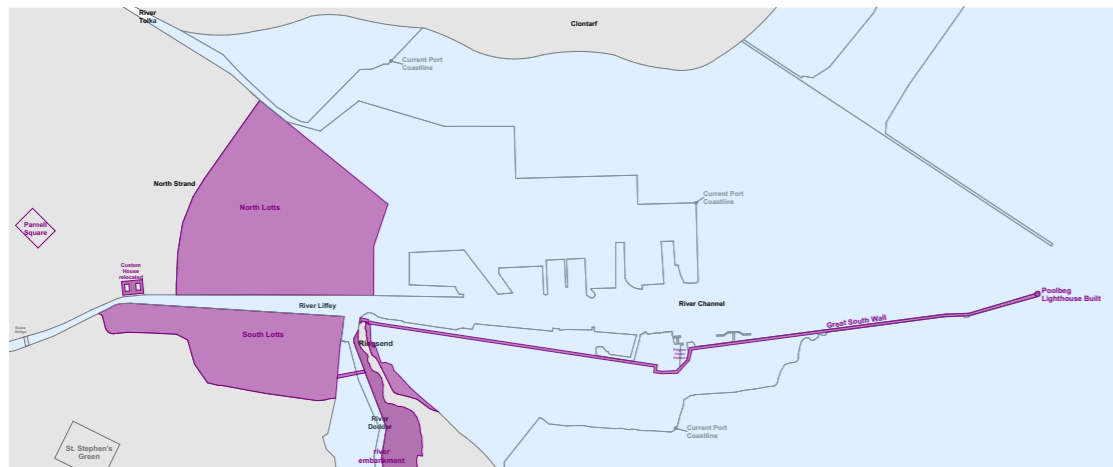
and a 24-foot wide deck surface. Timber coping was used in some places, but most of that has rotted and been replaced with cement.

The series of wall lengths extending east from the Dodder confluence to Poolbeg Lighthouse is known as the Great South Wall (GSW). While the GSW did not significantly reduce or scour Dublin Bar, it did protect the river channel from further deposition of sands from the South Bull. Acting as a training wall, it directed tidal currents in a more defined channel, contributing to tidal scour on the Bar when combined with the North Bull Wall (1819-1824). The GSW is a registered archaeological monument and a protected structure, with the identifiers RMP DU018-066 and DU019-029, and the reference number 6928.

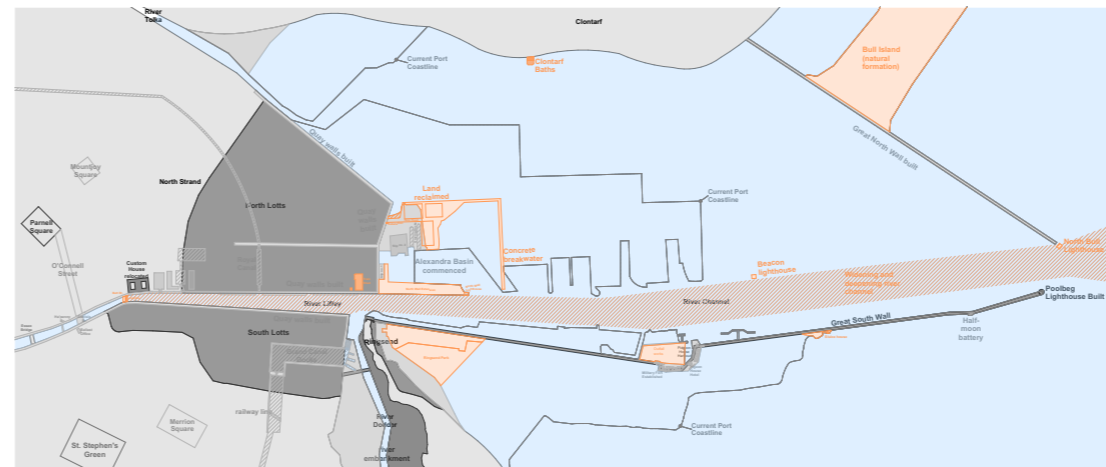




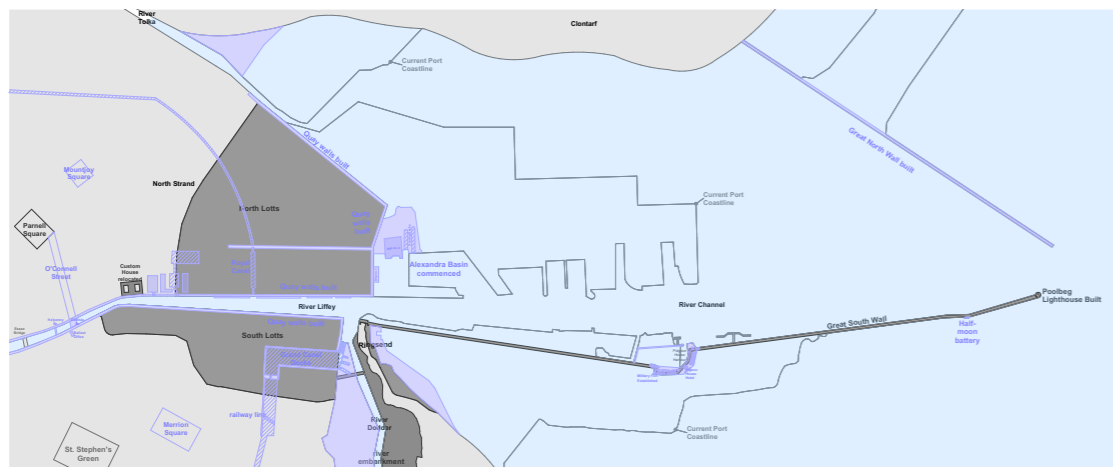
## Chronological Maps of Dublin Port's Development



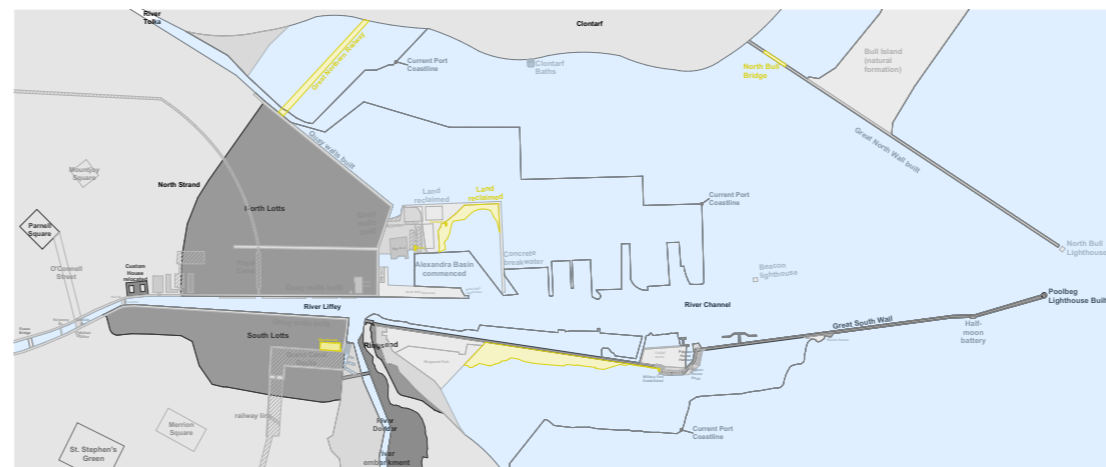
▲ 1707-1786 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP



▲ 1867-1898 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP



▲ 1786-1867 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP



▲ 1898-1902 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP

### 1707-1786

- Sir John Rogerson's Quay construction begins by 1716 and reclamation and construction of the South Lotts from 1723
- By 1791, the Pigeonhouse Harbour/Dock is planned
- Poolbeg Lighthouse opens in 1767

### 1786-1867

- Pigeon House Harbour completed and Half-Moon Battery built in 1793
- Grand Canal Dock works begin and completed in 1800
- Steam Shipping Arrives to Dublin in 1816
- Great North Wall built (1819-1824)
- George's Dock opens in 1821
- North Bull Wall completed in 1824

### 1867-1898

- Between 1868-98 Bindon Blood Stoney built the deep-water part along with rebuilding and deepening the quays
- Alexandra Basin operating as a wet dock, was established by the construction of the North Wall Quay extension between 1871-85

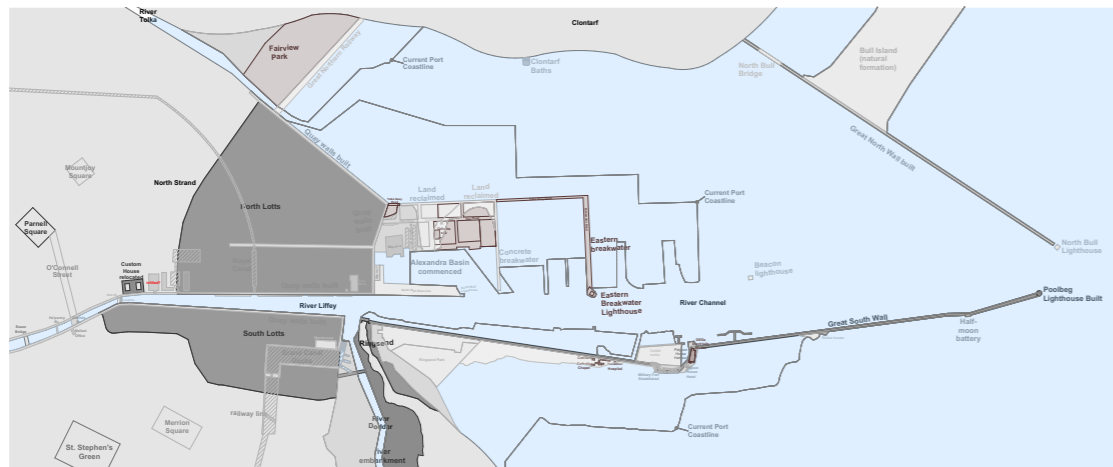
### 1898-1902

- Further eastward reclamation at the north port lands

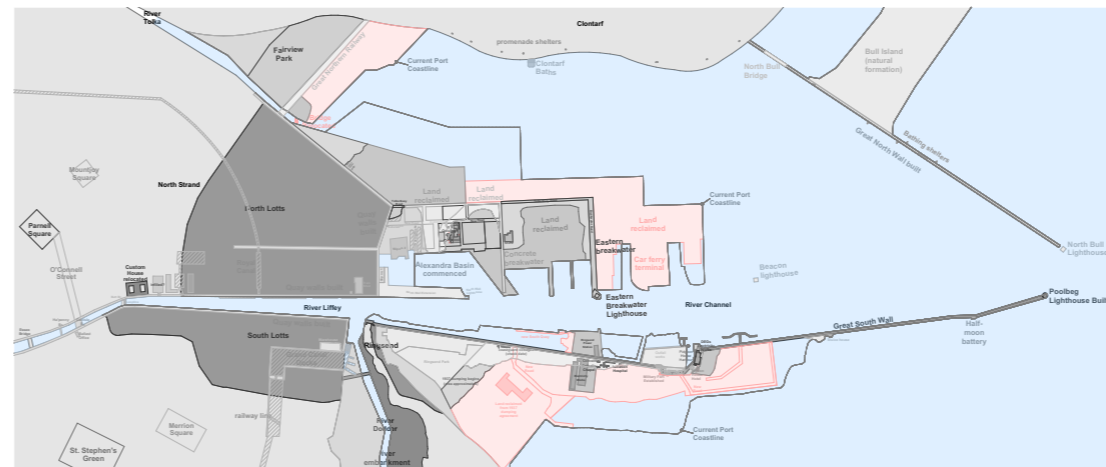




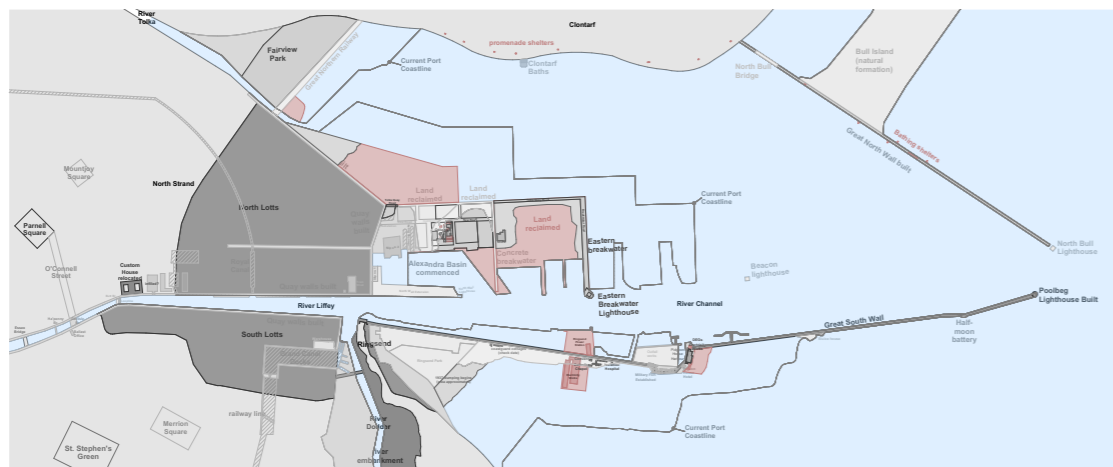
# Chronological Maps of Dublin Port's Development



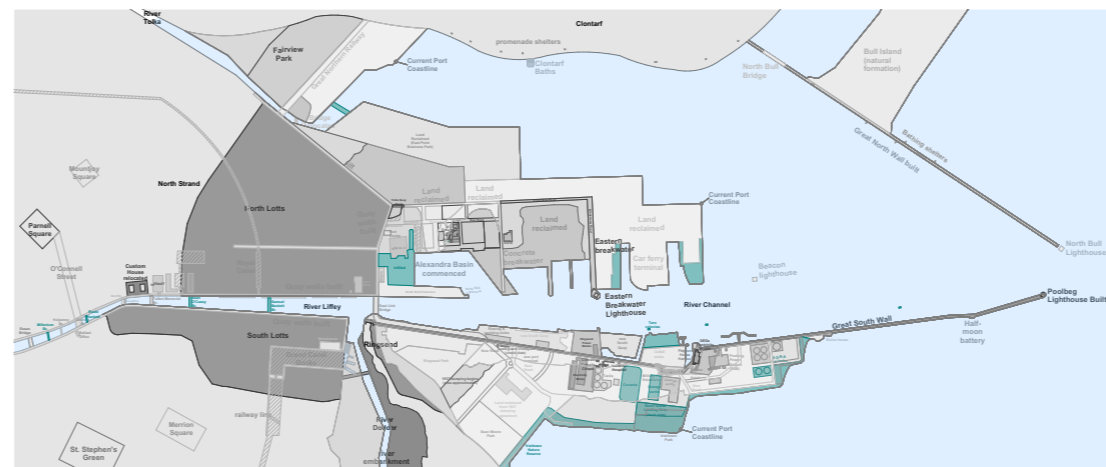
▲ 1902-1920 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP



▲ 1955-1968 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP



▲ 1943-1955 Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP



▲ 1986-Current Chronological Map  
Source: Grainne Shaffrey, Shaffrey Architects for DCP

## 1902-1920

- Pigeon House Power Station works begin
- Electrification of Dublin Port and docks in 1905
- Ongoing reclamation at Alexandra Basin and areas to the north

## 1943-1955

- The Harbour Act initiates further expansion
- Ringend Power Station constructed in 1955
- Ocean Pier development in 1953-1959

## 1955-1968

- Dublin Port start negotiations to purchase 650 acres of foreshore south of the Pigeon House Road and Great South Wall

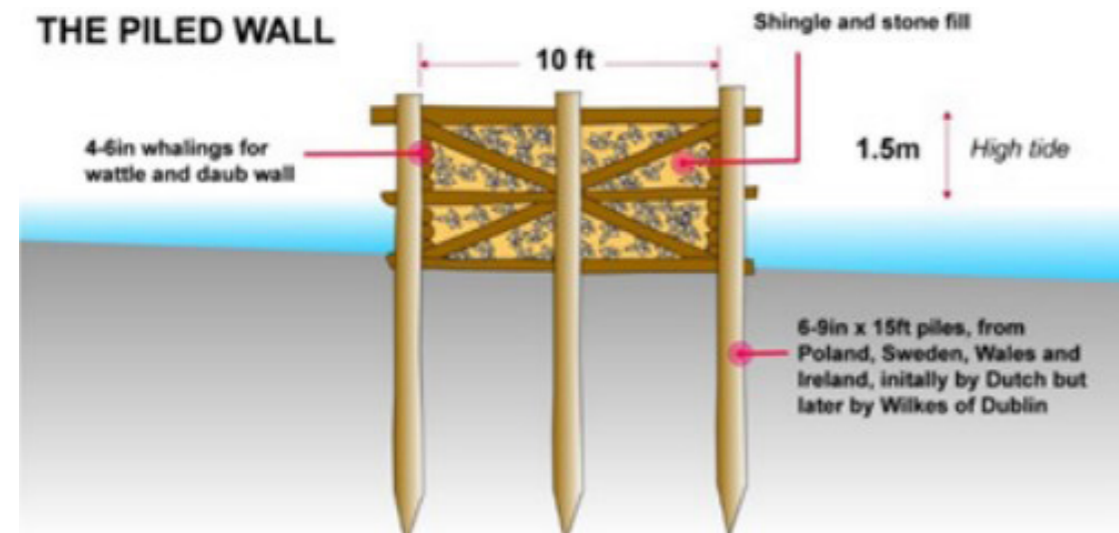
## 1986-Current

- Dublin Port Company established in 1997
- Dublin Waste-To-Energy begins operation in 2017



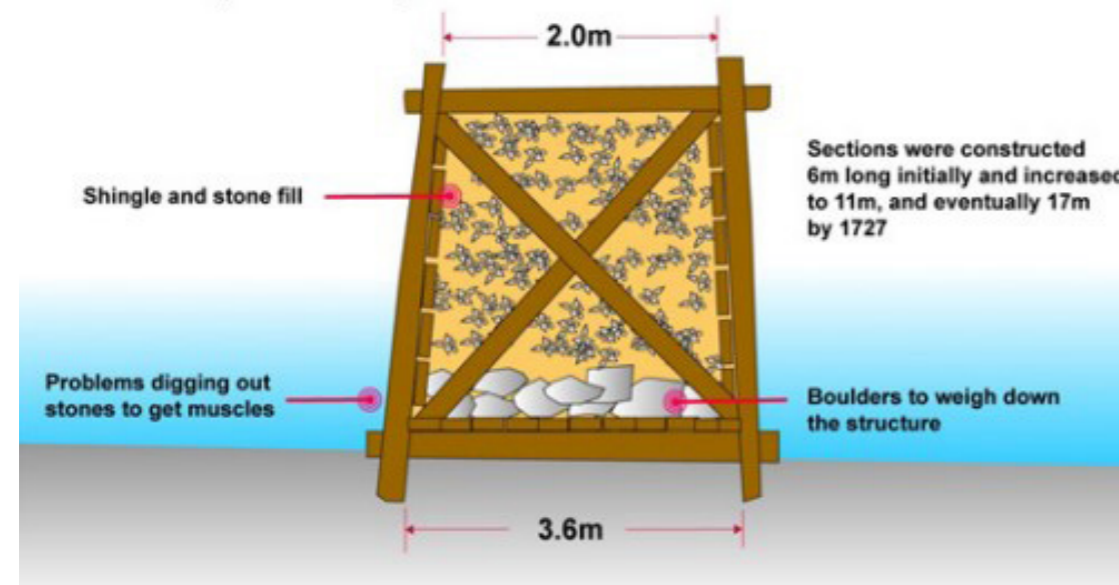


## Construction Methods of The Great South Wall

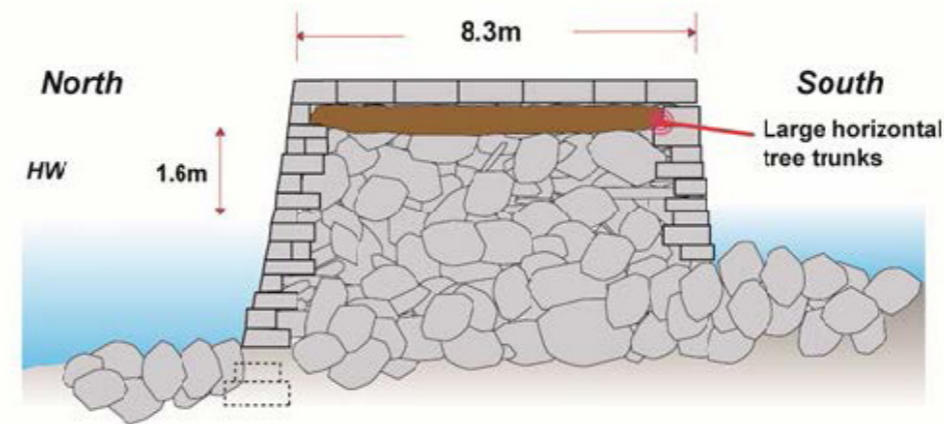


▲ Construction of piled wall based on contemporary descriptions  
Source: Southgate for DCP

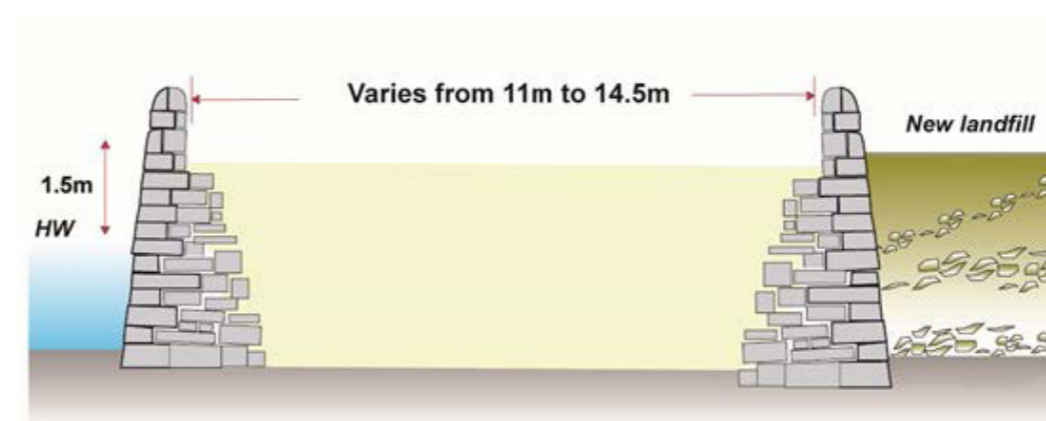
### THE PILES (1721-1731)



▲ Construction of The Piles 1721-1731  
Source: Southgate for DCP



▲ Cross section between Half-moon Battery and Poolbeg Lighthouse  
Source: Southgate for DCP



▲ Cross section between Pigeon House precinct and Ringsend ( Drawn at exposed section near disused sewerage outfall)  
Source: Southgate for DCP

The Piled Wall was initiated in the year 1715 by the Ballast Office, extending from Corn Exchange Place to Poolbeg. Before 1715, the waterway that leads to Dublin Port between the North Bull and the South Bull was maintained free from obstruction through basic dredging practices.

The construction of wall sections in Ringsend through prefabrication was marked by persistent challenges, incidents, and ongoing repairs, yet it was finished by 1731. The exact location of this construction remains uncertain, but it is believed to have been adjacent to the piled section in the north. The difficulty in securing the light ship at Poolbeg led to the construction of the first lighthouse in 1767. The Piles project, managed by the Ballast Office, was ultimately abandoned in 1759 due to damage caused by storms and tidal forces, which resulted in reported rotting and stress. The details of the construction are derived from contemporary accounts.

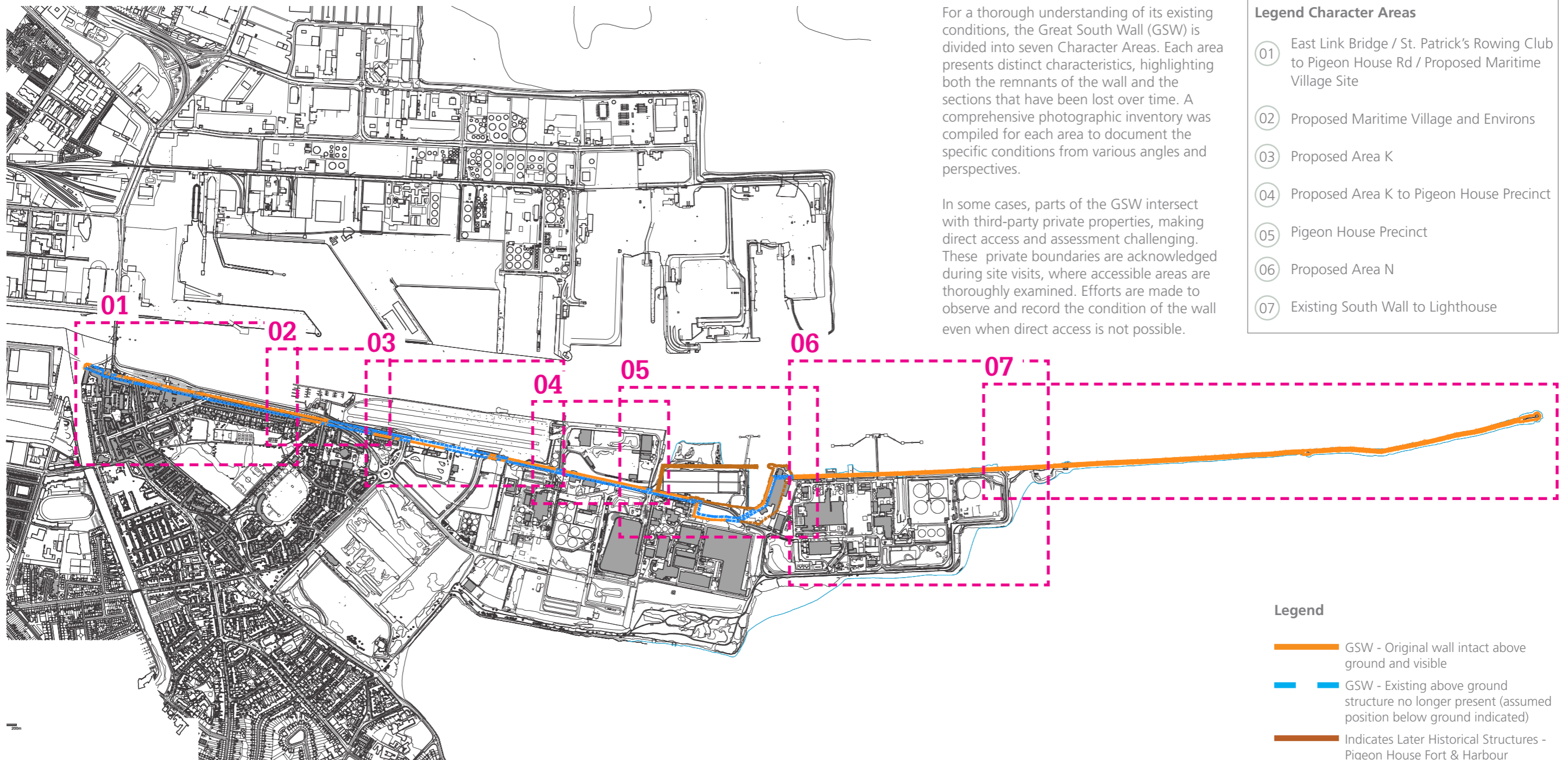
During the period from 1731 to 1733, a plan was put forward to extend the wall as a double stone wall in the direction of Sandymount, given that the water was relatively shallow at around 700mm deep. However, by 1760, this construction had fallen into a state of disrepair. Unfortunately, there is a lack of comprehensive documentation regarding the specifics of the construction.

In 1759, the Ballast Office made the decision to extend the double masonry wall from the East abutment at Poolbeg to the newly built Ballast Office Wall. John Smith of Dublin oversaw the construction, which progressed slowly, with only a 500m section finished by 1787. The construction was ultimately completed between 1792 and 1795, with the addition of the Half Moon battery in 1793. Historical records indicate that the wall was erected to the north of The Piles, a location consistent with photographs taken after The Piles were exposed following a severe storm in 1981.



## Section 02 - Existing Condition of the Great South Wall

### Existing Site Plan

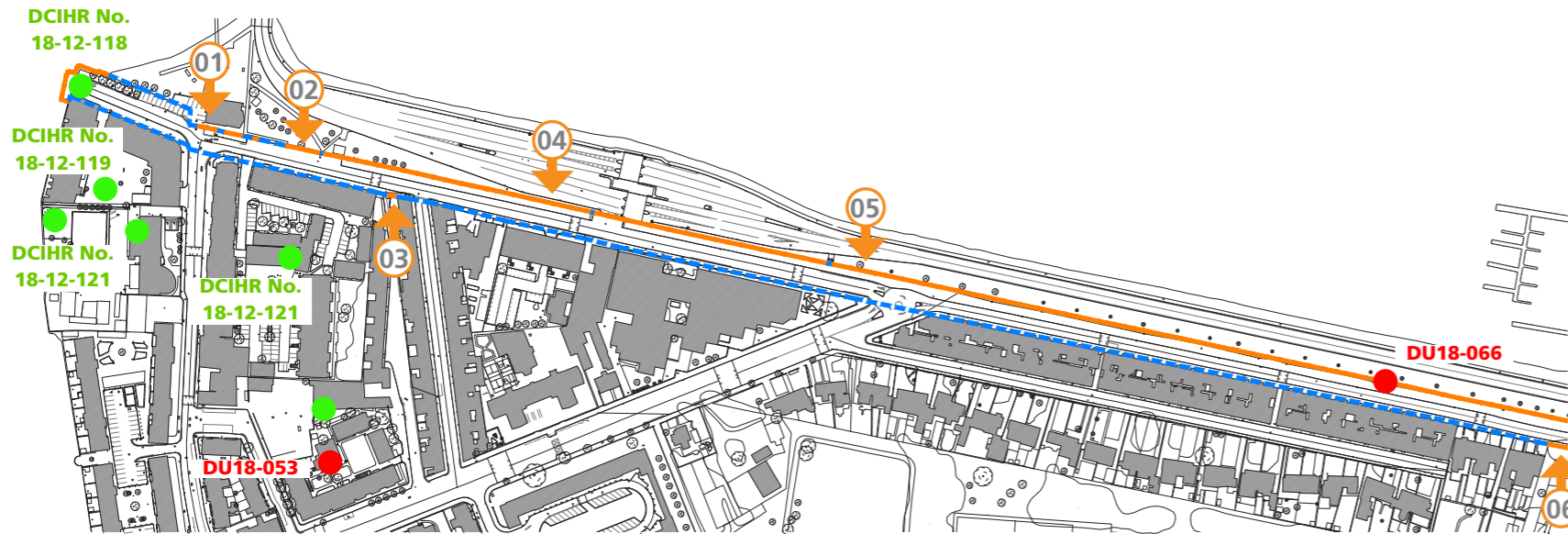


Existing Site Plan of the Great South Wall





Existing Site Plan - Area 1 of Great South Wall: East Link Bridge / St. Patrick's Rowing Club to Pigeon House Rd / Proposed Maritime Village Site



Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage
- Dublin City Industrial Heritage Record
- Record of Protected Structures
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)

- ▲ Existing area 1 of the Great South Wall
- ▼ **03.** Section of the Great South Wall forming a boundary to a residential private amenity space at Pembroke Cottages

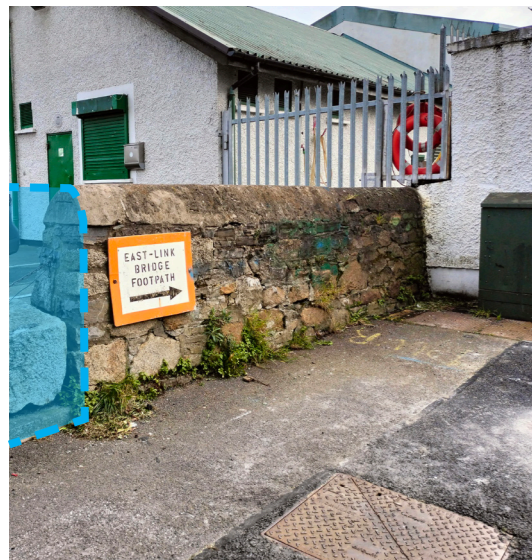


▲ Keyplan showing extent of the Great South Wall



▲ **05.** Section of the Great South Wall with segment removed for access route connecting Pigeon House Road and R131

- ▲ **06.** Section of the Great South Wall forming a boundary to Ringend Park and Pigeon House Road
- ▼ **04.** Section of the Great South Wall forming a boundary to York Road and R131



▲ **01.** Section of the Great South Wall partially forming a boundary to St. Patrick's Rowing Club. The wall continues behind existing structure



▲ **02.** Section of the Great South Wall with segment removed for access route connecting public park and Fishermans Wharf residential.



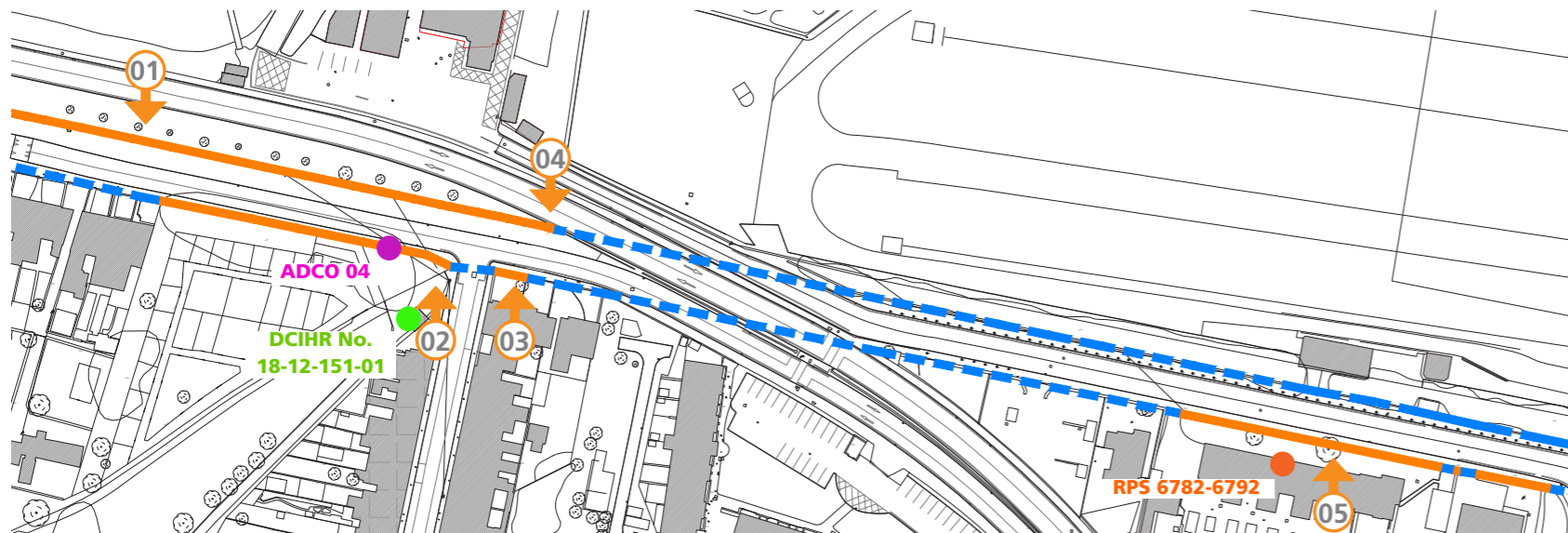
— Great South Wall Missing Portions



Section 02



Existing Site Plan - Area 2 of Great South Wall: Proposed Maritime Village and Environs



Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage
- Dublin City Industrial Heritage Record
- Record of Protected Structures
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)

- ▲ Existing area 2 of the Great South Wall
- ▼ **02.** Section of the Great South Wall forming a boundary to Pigeon House Road and Ringsend Park



▲ Keyplan showing extent of the Great South Wall



▲ **05.** Section of the Great South Wall forming a boundary to residential terrace on Pigeon House Road

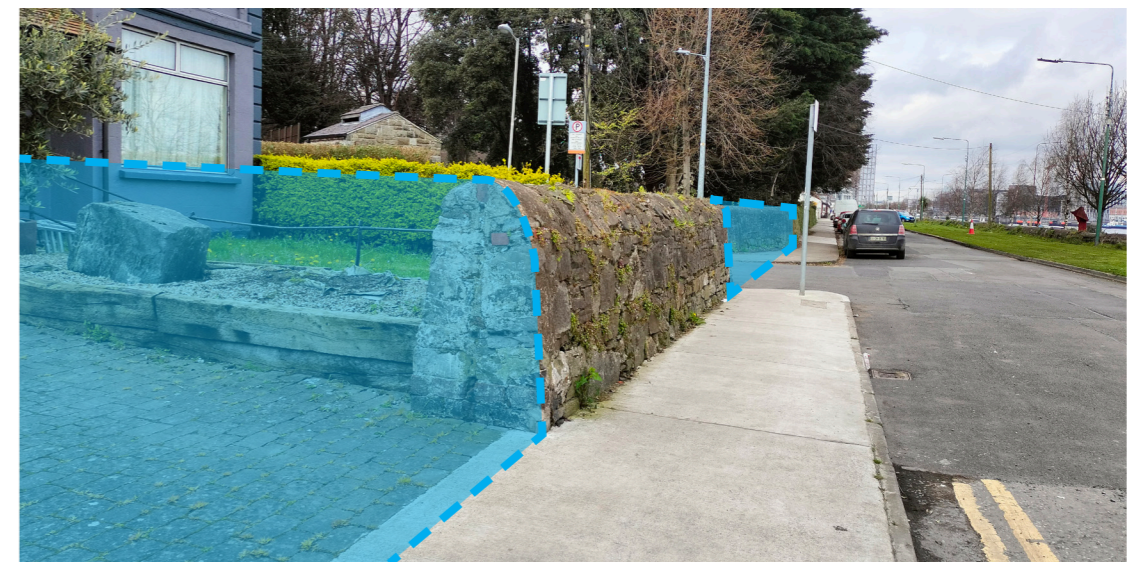
- ▼ **04.** Section of the Great South Wall with modern concrete segment forming a boundary to Pigeon House Road and R131
- ▼ **03.** Section of the Great South Wall forming a boundary to Pigeon House Road and Cambridge Avenue residential development



▲ **01.** Section of the Great South Wall forming a boundary to Pigeon House Road and R131 adjacent to Stella Maris Rowing Club



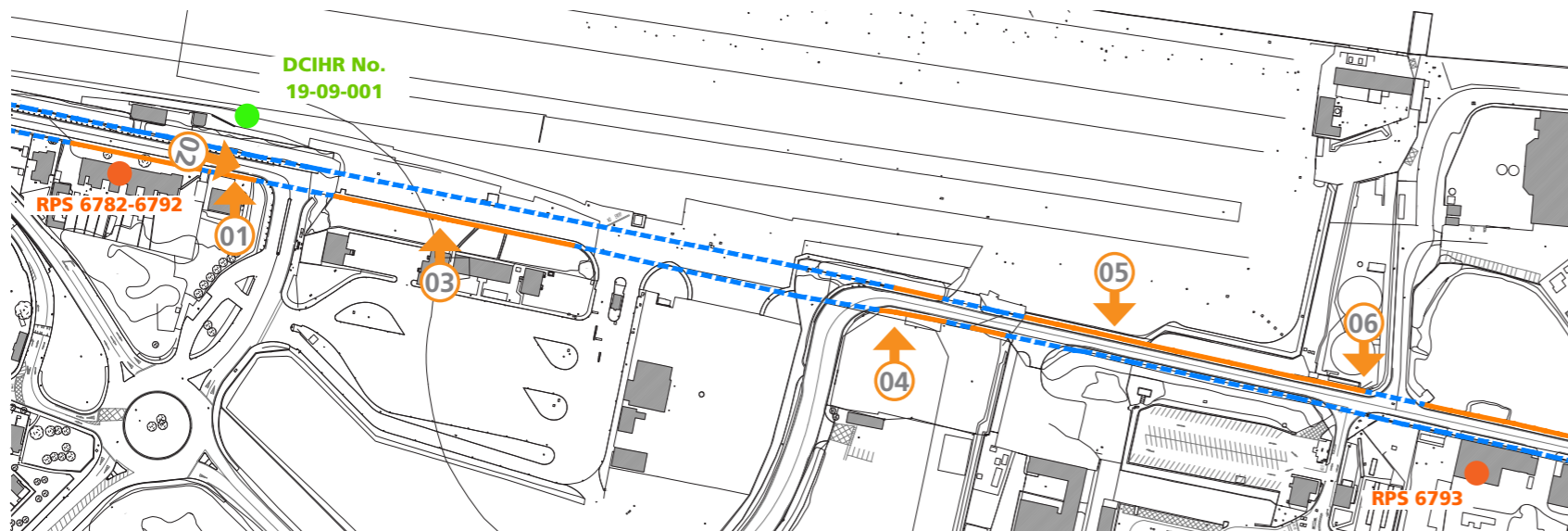
— Great South Wall Missing Portions



Section 02



Existing Site Plan - Area 3 of Great South Wall: Proposed Area K



- Refer to **3FM EAIR Chapter 16 Appendices**
- National Inventory of Architectural Heritage
  - Dublin City Industrial Heritage Record
  - Record of Protected Structures
  - ADCO

- GSW - Original wall intact above ground and visible
- - - GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- Indicates later historical structures
- ▲ Existing area 3 of the Great South Wall
- ▼ 02. Section of the Great South forming a boundary to 1st Port of Dublin Ringsend Sea Scouts with large segments missing on Pigeon House Road leading onto Sean Moore Road



▲ 01. Section of the Great South Wall forming a boundary to 1st Port of Dublin Ringsend Sea Scouts on Pigeon House Road



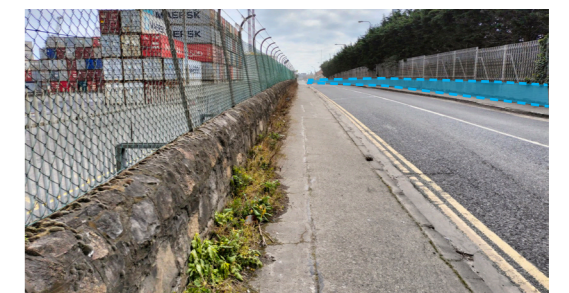
- - - Great South Wall Missing Portions



▲ 06. Section of the Great South Wall forming a boundary to the entrance of Hammond Lane Metal Recycling with segment missing on Pigeon House Road



▲ Keyplan showing extent of the Great South Wall



▲ 05. Section of the Great South Wall forming a boundary to MTL Container Terminal with adjacent wall missing on Pigeon House Road



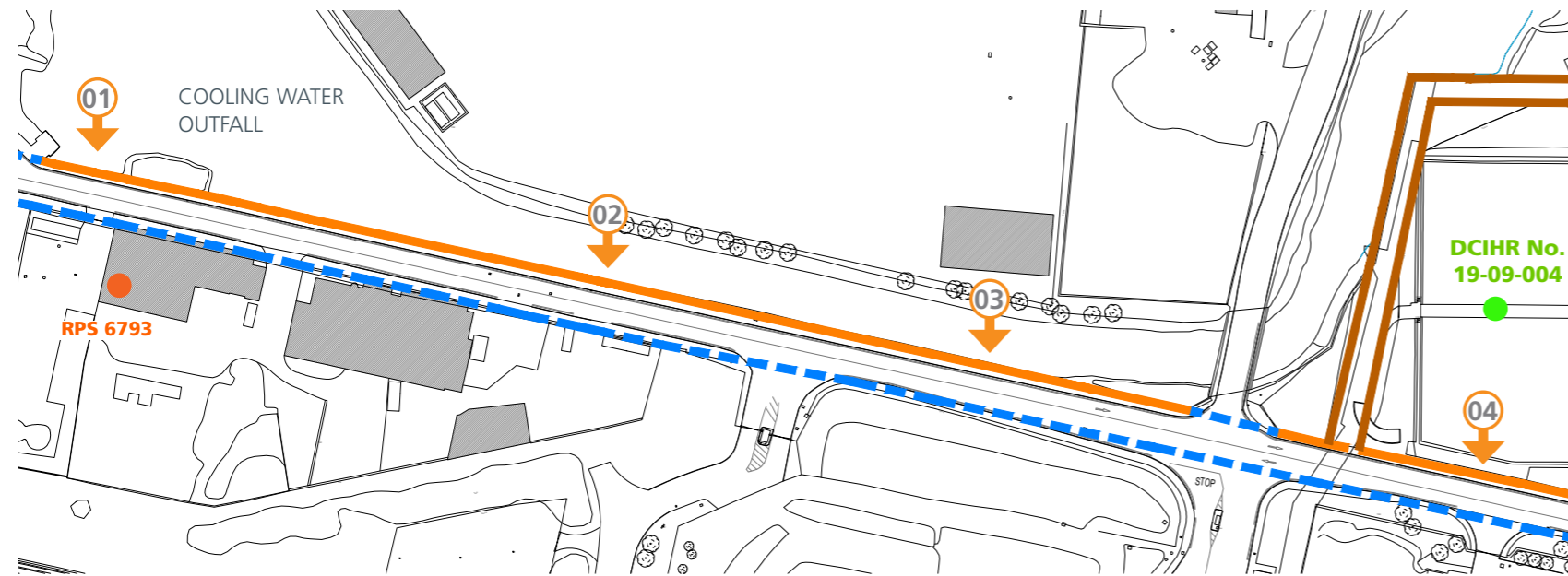
▼ 03. Section of the Great South Wall with segment missing forming a boundary to John Nolan Transport on Pigeon House Road



Section 02



Existing Site Plan - Area 4 of Great South Wall: Proposed Area K to Pigeon House Precinct



- Refer to **3FM EAIR Chapter 16 Appendices**
- National Inventory of Architectural Heritage
  - Dublin City Industrial Heritage Record
  - Record of Protected Structures
  - ADCO

- GSW - Original wall intact above ground and visible
- - - GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- Indicates later historical structures
- ▲ Existing area 4 of the Great South Wall
- ▼ **02.** Section of the Great South Wall forming a boundary to Hammond Lane Metal Recycling on Pigeon House Road



▲ Keyplan showing extent of the Great South Wall

**Proposed Area K to Pigeon House Precinct**

This stretch of Pigeon House Road forms the current port industrial landscape, following the alignment of the Great South Wall. A significant portion of the northern parapet remains intact along the cooling water outfall, allowing its full height to be appreciated next to the water. In contrast, most of the southern parapet has been destroyed and is no longer visible above ground. Instead, various boundary treatments now follow the former alignment, enclosing industrial sites to the south of Pigeon House Road.



▲ **04.** Section of the Great South Wall forming a boundary to Ringsend Waste Water Treatment Plant on Pigeon House Road

▼ **03.** Section of the Great South Wall forming a boundary to Hammond Lane Metal Recycling on Pigeon House Road

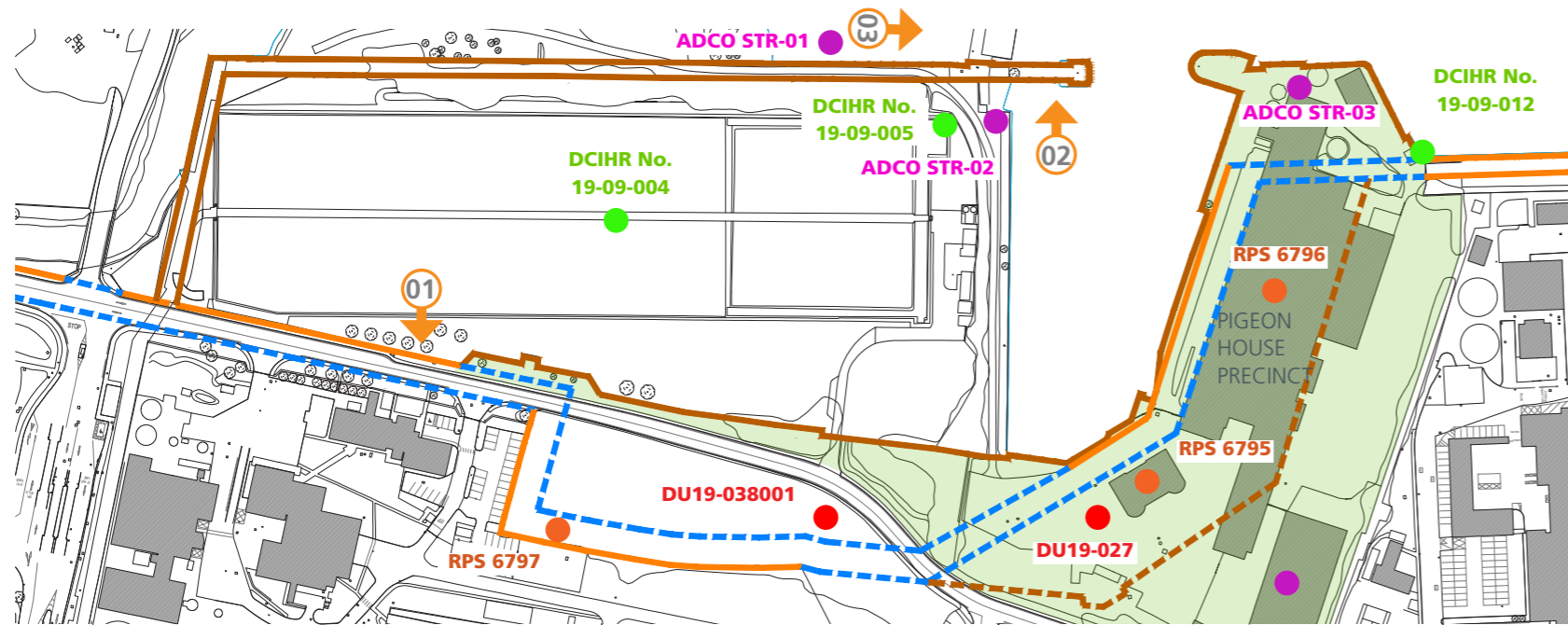


▲ **01.** Section of the Great South Wall forming a boundary to Hammond Lane Metal Recycling on Pigeon House Road





Existing Site Plan - Area 5 of Great South Wall: Pigeon House Precinct



Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage
- Dublin City Industrial Heritage Record
- Record of Protected Structures
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- Indicates later historical structures
- ▲ Existing area 5 of the Great South Wall
- ▼ 01. Section of the Great South Wall with modern concrete segment forming a boundary to Ringsend Waste Water Treatment Plant on Pigeon House Road



▲ View of The Pigeon House



Great South Wall Missing Portions



▲ 03. Section of the Great South Wall looking east towards The Pigeon House



▲ Keyplan showing extent of the Great South Wall

**Pigeon House Precinct**

The Pigeon House Precinct is a historically rich area bounded by the Harbour, GSW / Pigeon House Road, and the Fort's enclosing wall. Public access significantly diminishes nearing the precinct, with the northern parapet of the Great South Wall visible while the southern parapet is hidden by a new wall separating private properties. The Great South Wall's previously linear path alters direction near the fort ruins. The wall disappears within ESB lands and eventually reappears, often visible along the water's edge but hidden from the footway. Predominantly accessed by port and industrial traffic, the area sees low pedestrian activity.

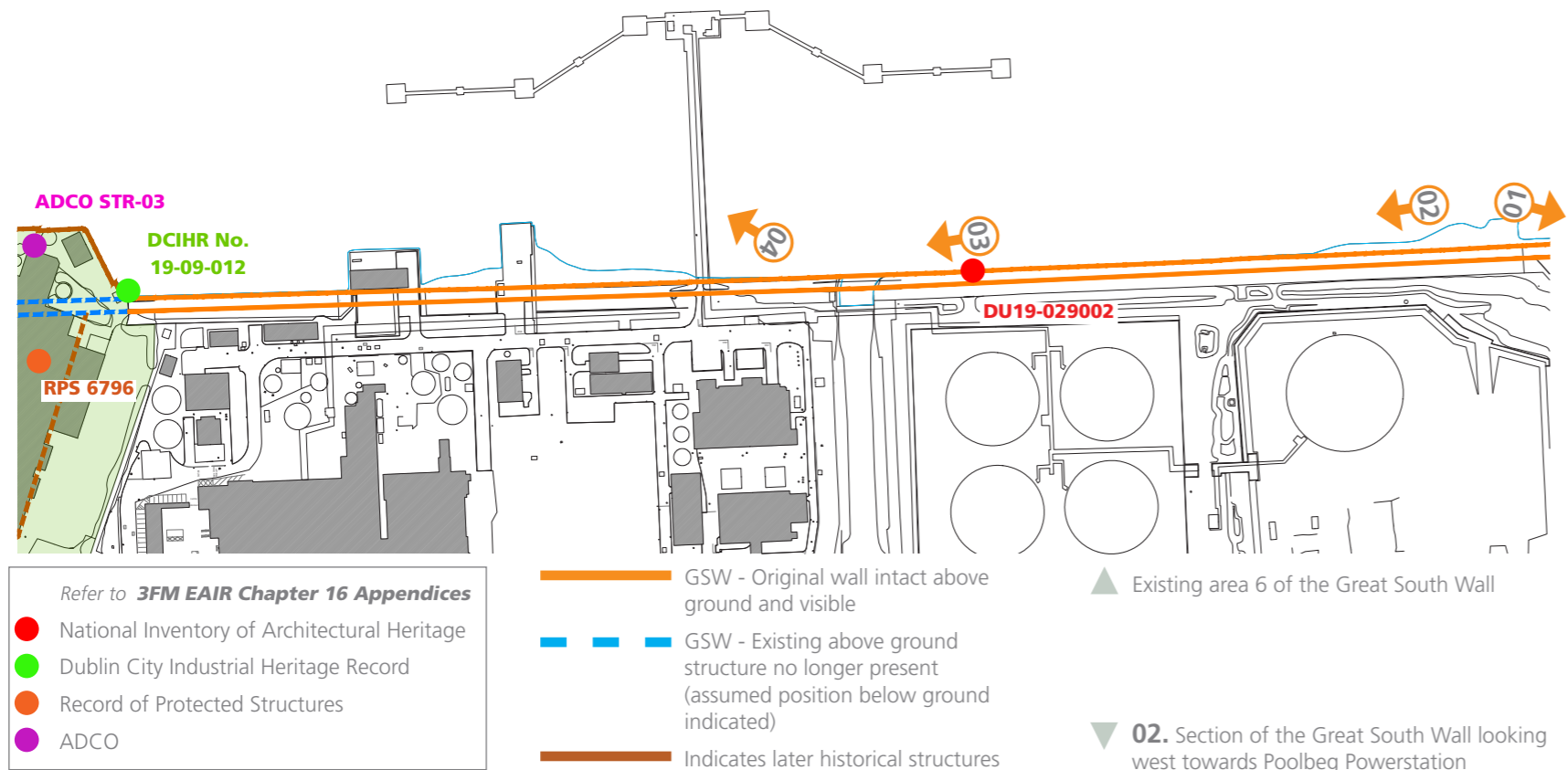


▼ 02. Section of the Great South Wall looking north from The Pigeon House





Existing Site Plan - Area 6 of Great South Wall: Proposed Area N



Pigeon House Precinct



01. Section of the Great South Wall looking east towards Poolbeg Lighthouse from Poolbeg Powerstation



02. Section of the Great South Wall looking west towards Poolbeg Powerstation



03. Section of the Great South Wall looking west towards Poolbeg Powerstation



04. Section of the Great South Wall looking north from Poolbeg Powerstation



Keyplan showing extent of the Great South Wall

Proposed Area N

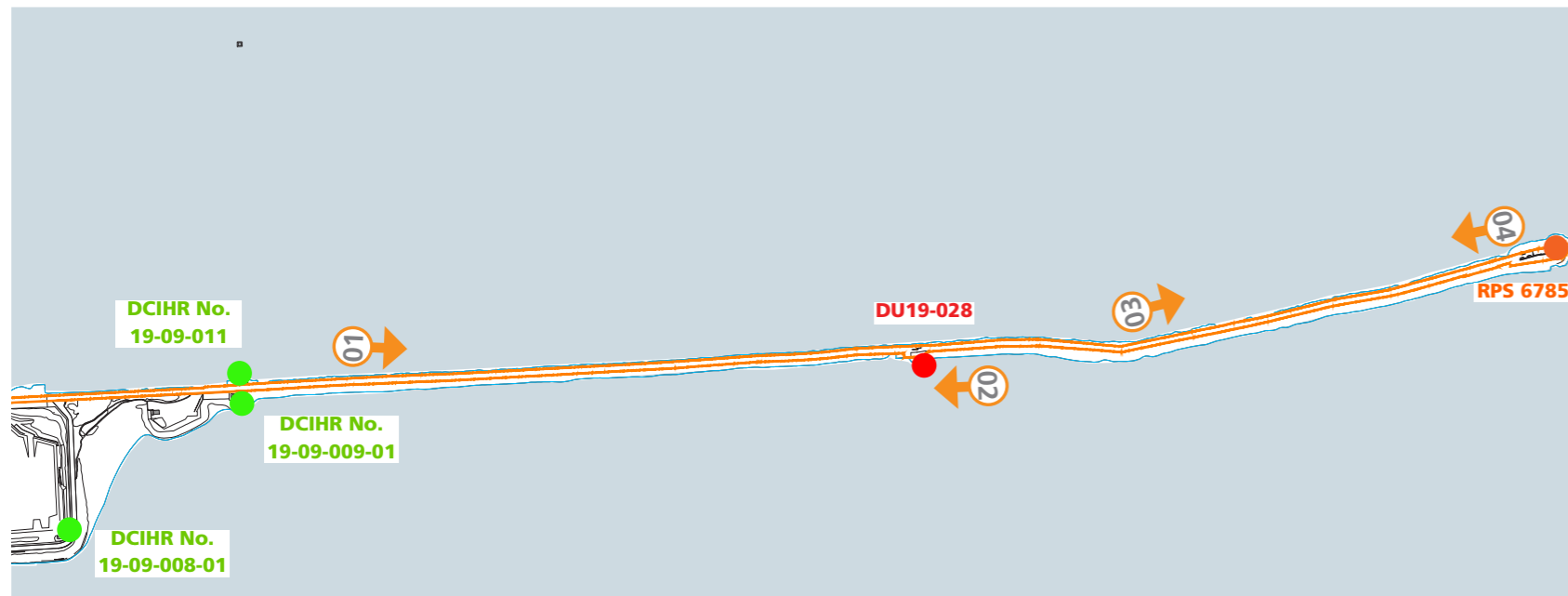
The area under consideration includes a section of the Great South Wall, currently inaccessible to the public and owned by the Dublin Port Company. The GSW reappears along the water's edge after disappearing within ESB lands. This area currently experiences low pedestrian activity and can only be viewed through the fence on the other side.



Section 02



Existing Site Plan - Area 7 of Great South Wall: Existing South Wall to Lighthouse



Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage
- Dublin City Industrial Heritage Record
- Record of Protected Structures
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- ▲ Existing area 7 of the Great South Wall
- ▼ 01. Section of the Great South Wall looking east towards Poolbeg Lighthouse



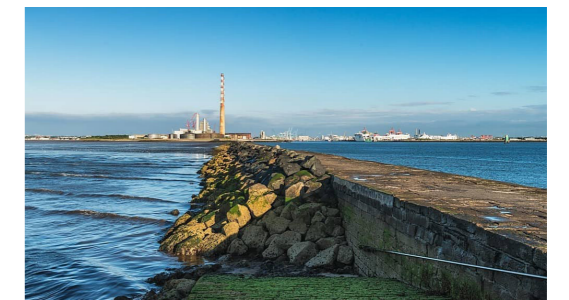
▲ Aerial view of Poolbeg Lighthouse and the Great South Wall



▲ 04. Section of the Great South Wall and cast iron crane looking west towards Poolbeg Powerstation



▲ Keyplan showing extent of the Great South Wall



▲ 03. Section of the Great South Wall looking west from slipway towards Poolbeg Powerstation



▼ 02. Section of the Great South Wall looking east towards Poolbeg Lighthouse



Section 02



## Section 03 - Proposed Impacts, Mitigation & Interpretation of the Great South Wall

### Summary of Intent

#### Overall Intent

The proposed initiatives outlined in the following pages aim to restore the legibility and historical route of the Great South Wall, which has become obscured over time due to various interventions related to port and industrial development in the area. As part of the 3FM Project, further impacts on the wall are anticipated to facilitate infrastructural works. To mitigate these effects, several strategies will be employed. These include a comprehensive conservation plan for repairing, restoring, and reconstructing sections of the wall, as well as proposals for an interpretation strategy and concept for the entire length of the wall.

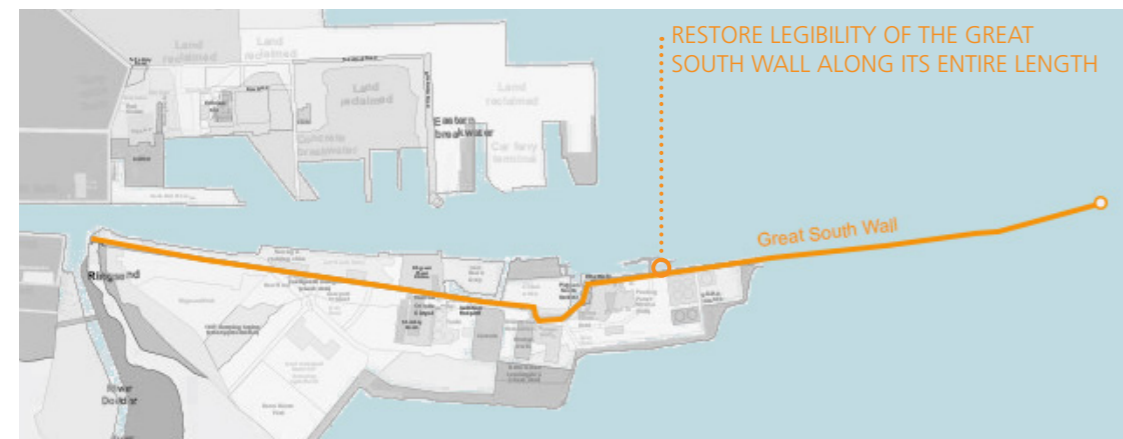
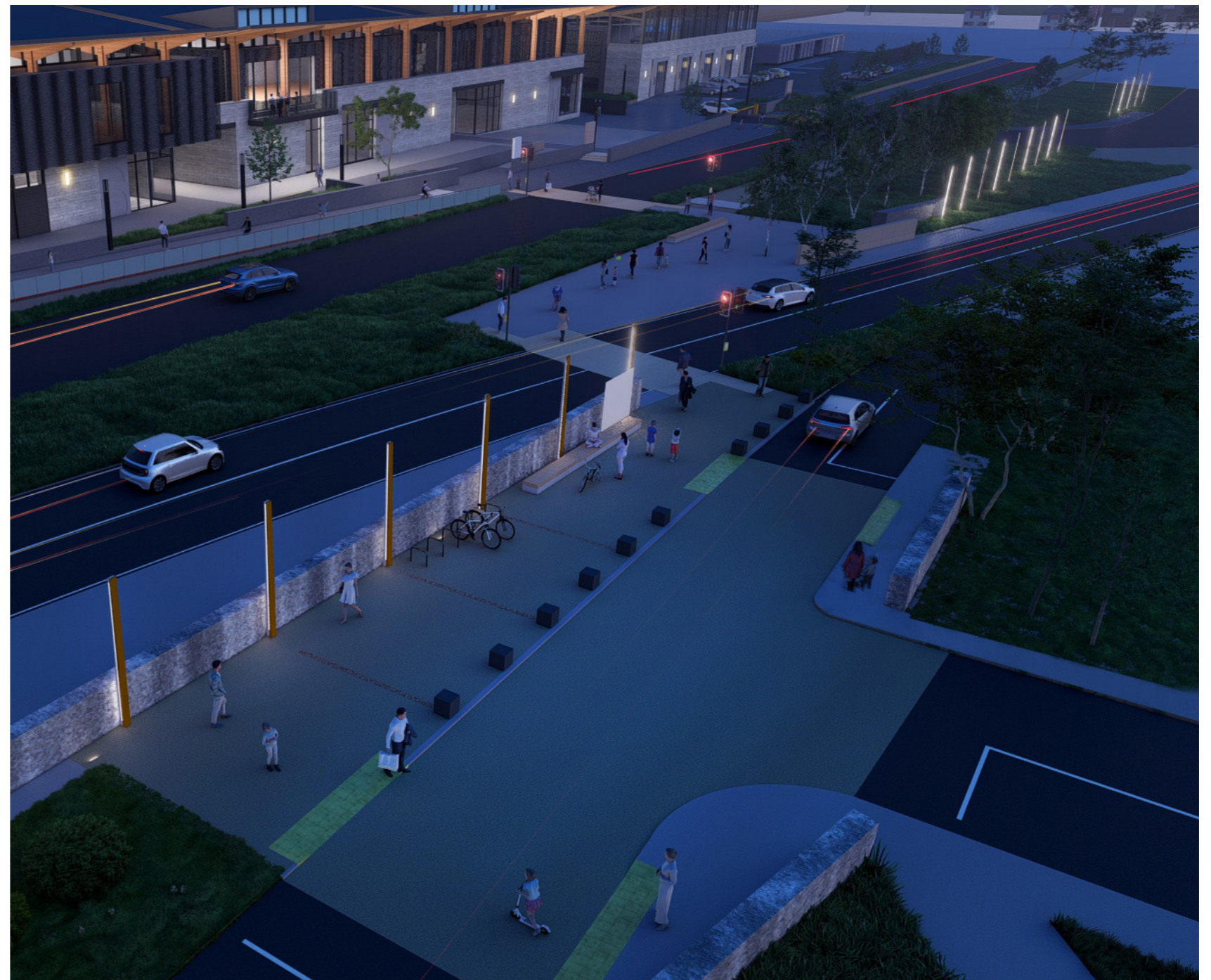
Through the use of markers, totems, and interpretive elements, the route of the wall will be highlighted and reintroduced into public awareness. Additionally, any unavoidable disturbances to the wall as part of the 3FM Project will be mitigated with a range of suggested measures.

The Interpretation Strategy for the Great South Wall project focuses on several key elements to enhance visitor experience and preserve historical integrity. Vertical markers,

will delineate the original wall's route at 30-metre intervals, providing information location. Special totems, larger in scale, will offer interpretive information and highlight directional changes, maintaining a cohesive visual language tailored to specific contexts.

Further measures include integrating salvaged stone into the landscape and totems to preserve & repurpose historical elements. Ground surface marking, including differentiated paving surfaces and stainless steel studs, will further emphasize the wall's historical route, seamlessly integrating with existing and proposed infrastructure.

Overall, these design elements aim to create a visually consistent experience, guide people along the historic route, and uphold conservation principles. These initiatives will significantly benefit public realms such as areas along the proposed maritime village by enhancing its historical and cultural narrative. The interpretive elements can create a vibrant and engaging environment, by blending the old with the new in a way that respects and celebrates its past.



Aerial view at night towards the proposed Maritime Village with proposed interpretive elements of the Great South Wall



## Overview of Impacts & Proposed Mitigation

**A significant portion of the Great South Wall's route from Ringsend to the beginning of the publicly accessible Pigeon House Walk traverses the industrial and infrastructural landscape of Dublin's Southern Port lands. The proposed 3FM project focuses on the Dublin Port Company-owned lands on the Poolbeg Peninsula, which comprise one-fifth of the Dublin Port estate.**

The proposed works as part of the 3FM project intersect with the Great South Wall at various points due to the close proximity of the wall in certain areas. Throughout the design process, every effort has been made to minimize impacts on the Great South Wall, particularly on the remaining visible sections. The engineering team at RPS, along with the Conservation & Archaeology Team from ADCO, Southgate Associates, MOLA Architecture, and Shaffrey Architects, have collaborated to ensure any impacts are minimized or, where unavoidable, managed under strict conservation oversight according to the following key policies:

### Recording Prior to Works:

The Great South Wall, a structure of international technical significance, has been recorded according to established guidelines.

### Policies of Minimum Intervention:

Best practice involves necessary interventions while ensuring impacts are as limited as possible, helping to retain the original fabric of the structure in-situ whenever feasible. The Heritage team has enforced a policy of minimum intervention during the design development of the engineering proposals.

### Policies for Legibility in Case of Intervention:

Interventions required for the 3FM Project have been designed to ensure the path of the original Great South Wall remains legible. Any new work on the wall, whether restoration or reconstruction, is distinct from the original fabric, adhering to best conservation practices.

### Policies for Reversibility of Interventions:

The principle of reversibility has been adopted wherever possible to allow for future removal of interventions without damaging the original structure.

### Conservation Oversight:

The 3FM project design has been monitored by an expert heritage team, ensuring the preservation of both tangible and intangible heritage.

RPS Engineers, with input from Darmody Architecture and the Conservation Team, has prepared a series of key plans and drawings to identify areas of the wall that will be impacted. These drawings (**S26-HER-XX-DR-HE 100-1000 to 1006**) are included in the 3FM Application and should be reviewed in conjunction with this document. The key plans on the following pages offer an overview and are divided into seven zones, corresponding to both existing areas of distinct character and emerging character zones as part of the 3FM works. Please note that these key plans are intended for general reference and do not take precedence over the detailed RPS Drawings.

On these keyplans, the Great South Wall is identified under the following categories:

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet
- \*See notes - GSW - Proposed restoration of existing ground surface

As part of the mitigation strategy, particular areas have been identified where the above ground sections of the wall are to be restored and in some cases also fully reconstructed. These areas have been carefully identified and selected with guidance from the conservation team with the aim of improving the condition and legibility of the wall where possible.

Please note that a detailed strategy for conservation and repair of areas of the great South Wall is outlined in detail in **Chapter 16 of the EIAR: Cultural Heritage (Including Archaeological, Architectural & Industrial)** and a brief overview is provided within this document for information purposes only. In particular, reference should be made to the following section:

**Appendix 16-6 Specifications and Method Statement for Conservation and restoration of the Great South Wall parapets**

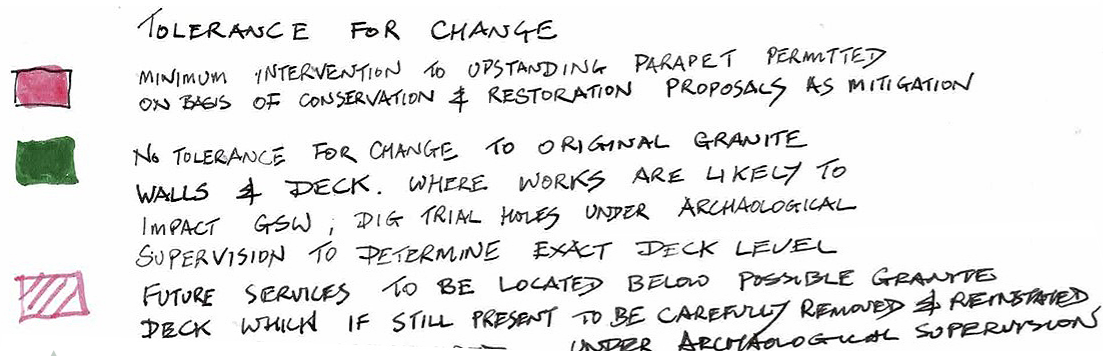
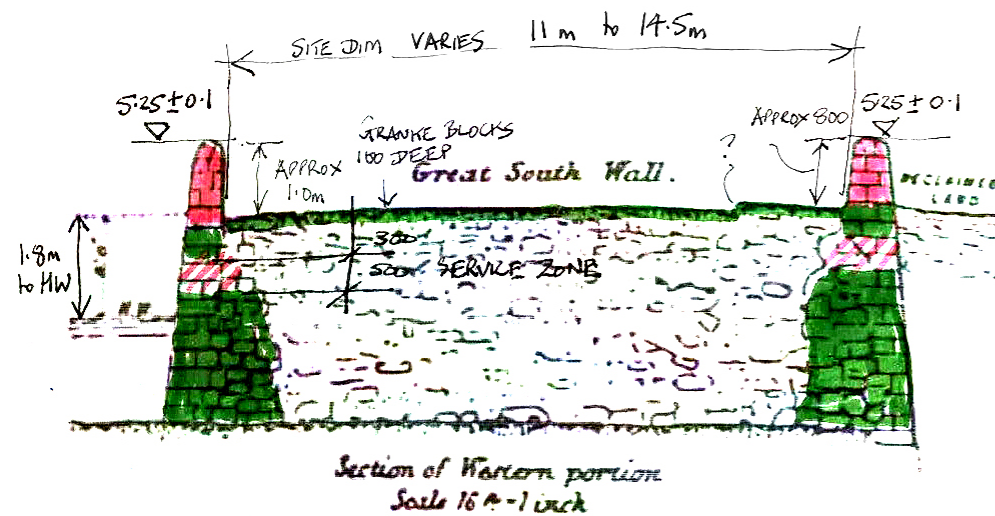


▲ Typical view of existing section of GSW in poor repair, courtesy of ADCO

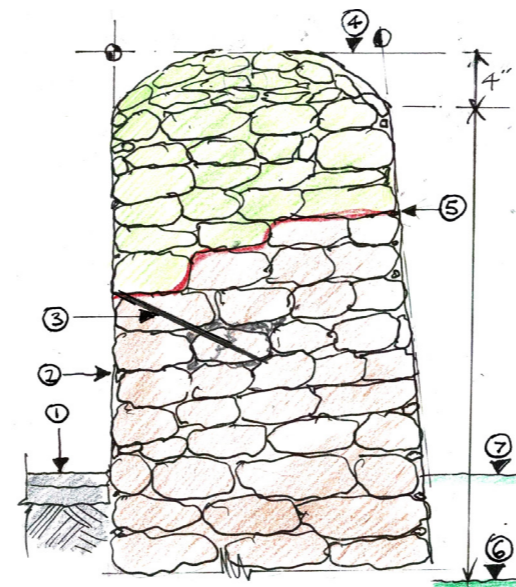


## Overview of Impacts & Proposed Mitigation continued

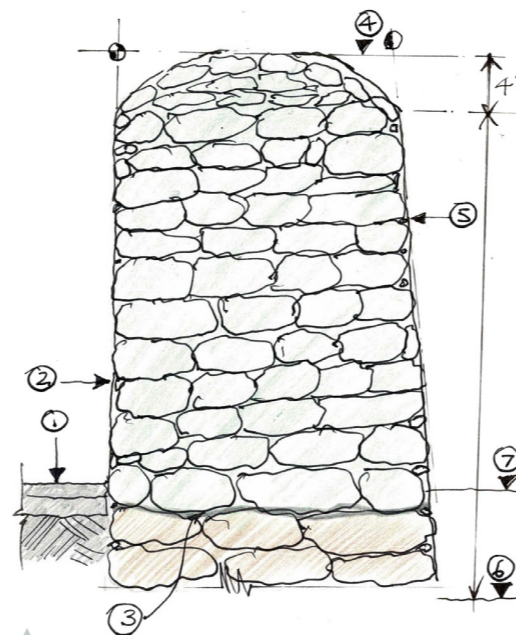
The diagrams and text on this page constitute extracts from Chapter 16 of the EIA including Appendix 16-6 (Specifications and Method Statement for Conservation and restoration of the Great South Wall parapets) and are provided for information purposes only. Please refer to EIA Document for full and detailed information.



Extract from EIA 16.6.3 outlining Pre-construction Phase measures including Archaeological Test Investigation required in all locations along the GSW to verify the levels and to ensure that the levels assumed in design are correct and that impacts to the GSW are minimized



Repairs to existing Wall - Typical Section (refer to EIA Chapter 16 for full details)



Typical Wall rebuild section- Typical Section (refer to EIA Chapter 16 for full details)

### Typical Repair Methodology - (refer to EIA Chapter 16 and Appendix 16-6 for full details):

1. New pathway to be resurfaced as shown on RPS drawings S26-HER-XX-DR-HE 100-1000 to 1006. These drawing also show position of repair and reconstruction of the GSW.
2. Carefully point the existing wall with NHL 3.5 lime mortar (Secil lime or similar approved with 5-7mm grit and pinning stones to match existing pointing grading. Carefully rebuild stones to exiting courses as described in the conservation methodology in appendix nn.
3. Where voids are encountered in existing masonry, grout with NHL 3.5 lime grout (Coulinex or similar approved) in accordance with the conservation methodology.

4. Set out the work to align the height (approx. 5.250 +/- 0,1m) with adjacent examples of original parapet and form the domed capping as shown to a rise of around 4 inches to match existing. Some areas of the wall have been rebuilt with a square stone coping, but this is not original and will not be followed for reconstruction or partial rebuilding of sections of the GSW.
5. Follow the batter and alignment of adjacent existing walls.
6. The original height was around 2 ft 8 in. to the South and 3 ft 6 in. to the North
7. Current levels are somewhat higher to account for road and footway mack up

### Typical Rebuild Methodology - (refer to EIA Chapter 16 and Appendix 16-6 for full details):

1. New pathway to be resurfaced as shown on RPS drawings S26-HER-XX-DR-HE 100-1000 to 1006. These drawing also show position of repair and reconstruction of the GSW.
2. Carefully rebuild wall in limestone to match exiting the existing wall in NHL 3.5 lime mortar (Secil lime or similar approved) with 5-7mm grit and pinning stones to match existing pointing grading. Carefully rebuild stones to exiting courses as described in the conservation methodology in appendix nn.
3. See recommendations for test trenching and establishing existing footing.
4. Set out the work to align the height

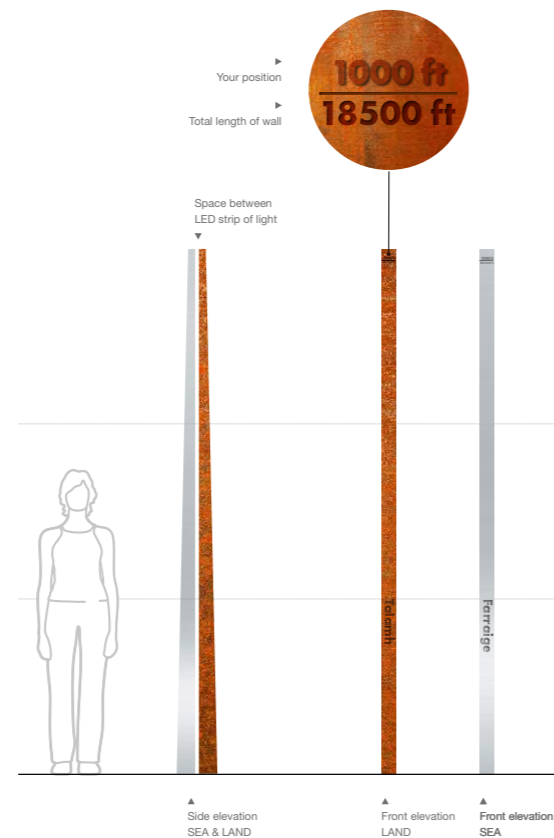
5. Follow the batter and alignment of adjacent existing walls.
6. The original height was around 2 ft 8 in into the South and 3 ft 6 in to the North
7. Current levels are somewhat higher to account for road and footway mack up



## Overview of Interpretation Concept Vertical Elements

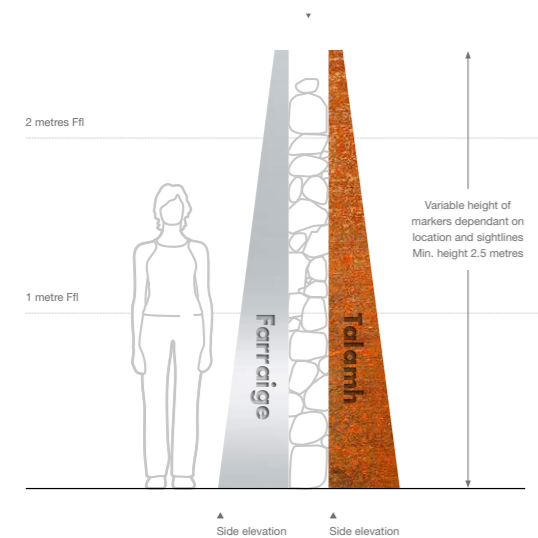
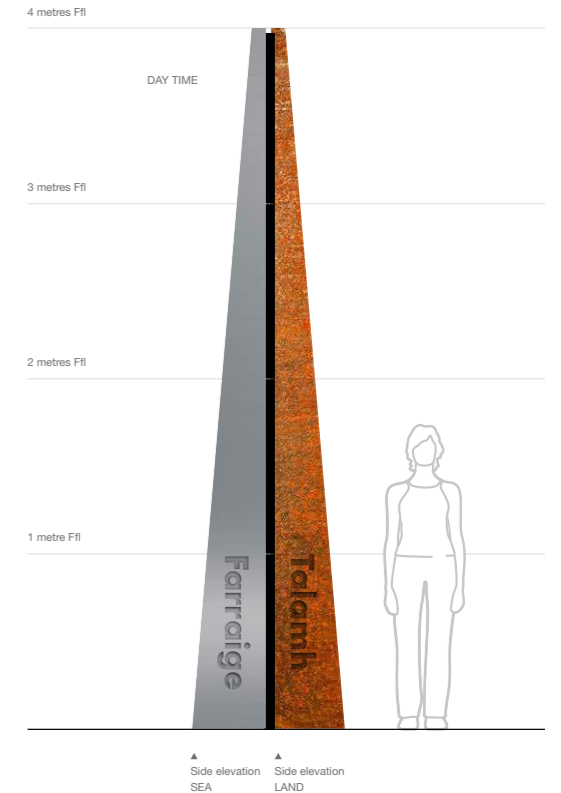
### Vertical Route Marking

A series of vertical markers are proposed to delineate the original route of the Great South Wall, creating a consistent visual element throughout. These markers are crafted from two materials: stainless steel to symbolize the sea side and Cor-Ten steel to represent the land side of the former wall. The compact footprint of the markers ensures they can be integrated seamlessly into various settings, even where space is limited. The markers will be positioned at approximately 30-metre intervals, with the option to increase density in certain areas to reinforce visual continuity where the wall's alignment is interrupted. Each marker will also feature information about the total length of the wall and the specific location of the marker along the route.



### Special Totems

A variety of special totem elements are proposed for key locations along the route. Larger in scale than the vertical markers, these totems will maintain a cohesive visual language. The totems will feature diverse design iterations tailored to their specific contexts, ensuring a consistent aesthetic throughout. They will provide additional interpretive information and highlight changes in the wall's direction, enhancing the overall narrative and guiding visitors along the historic route.



### Re-use of Salvaged Material

To address the unavoidable loss of small sections of the wall due to the 3FM proposals, salvaged stone from the demolished sections will be reintegrated into the landscape design and special totems. This salvaged stone will also be reused in the reconstruction or restoration of the GSW while ensuring a clear distinction between the old and new elements. This approach aligns with best practice conservation principles, preserving the historical integrity of the wall while incorporating it into the new design.



### Information Panels

At key locations along the wall, totem markers will feature graphic information panels providing historical and contextual details about the Great South Wall. These panels are designed to enhance both legibility and public awareness of the wall. The markers will be strategically placed at the beginning and end of the wall, as well as opposite the new Maritime Village, a high-traffic area expected to receive significant footfall.



## Overview of Interpretation Concept continued Other Elements

### Ground Surface Marking

In addition to vertical elements, the alignment of the wall will be demarcated on the ground surface where feasible. In areas of hard landscaping, particularly around special interpretation points, the wall's alignment will be subtly indicated with differentiated paving surfaces. Where possible, salvaged material from Dublin Port's existing stockpile, such as large granite slabs, will be repurposed as paving materials and seating elements. At points where the route intersects proposed new roadways, a series of stainless steel studs will mark its path. This ground-level approach will enhance the visibility of the wall's historical route while seamlessly integrating with both existing and planned infrastructure.



To further emphasize the historical significance, the differentiated paving surfaces will be designed to stand out subtly yet distinctly from the surrounding landscape. This could include variations in texture, color, or material, providing a tactile and visual cue to the wall's original path. The use of salvaged granite slabs not only reinforces the historical connection but also promotes sustainable practices by reusing existing resources.



By marking the wall's alignment at ground level, visitors will gain a clearer understanding of the original structure's extent and importance. This method of demarcation ensures that the historical route of the Great South Wall is preserved and highlighted within the modern urban context, offering an educational and engaging experience for the public.



### Graphic Panels

Graphic panels will be integrated into the interpretation proposals at several key locations. These panels will feature historical imagery, both photographic and artistic, to highlight the history of the wall and visually represent how the wall and its surrounding landscape may have appeared at various points in history. This approach will enhance the interpretation proposals, providing a richer layer of communication and legibility.

The execution of these panels will vary by location:

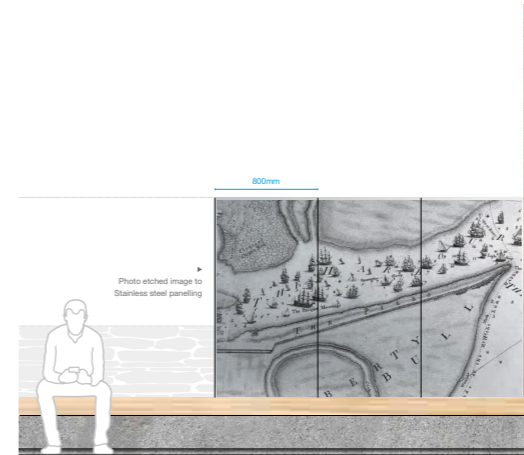
**Noise Barrier by SPAR Roadway:** Where a 4-metre high noise barrier is proposed to screen the new SPAR roadway from the residential properties along Pigeon House Road, external-grade vinyl with graphic reproductions of historic images of the Great South Wall will be applied to both sides. This will create a focal point for passersby, adding visual interest and historical context.

**Maritime Village:** At the Maritime Village, photo-etched stainless steel panels will be installed alongside the vertical markers. These panels will provide durable and detailed historical images, complementing the site's interpretive elements.

**Pigeon House Precinct:** In the Pigeon House Precinct, stainless steel panels with perforated designs will be incorporated into the fence design. These panels will depict images of the former fort area and will remain transparent, allowing glimpses of the Precinct behind the fence. This design will blend historical representation with modern aesthetics, maintaining visual connection to the site.

These graphic panels will significantly

enhance the interpretation of the Great South Wall, providing historical insights and fostering a deeper appreciation of the site's heritage among visitors.

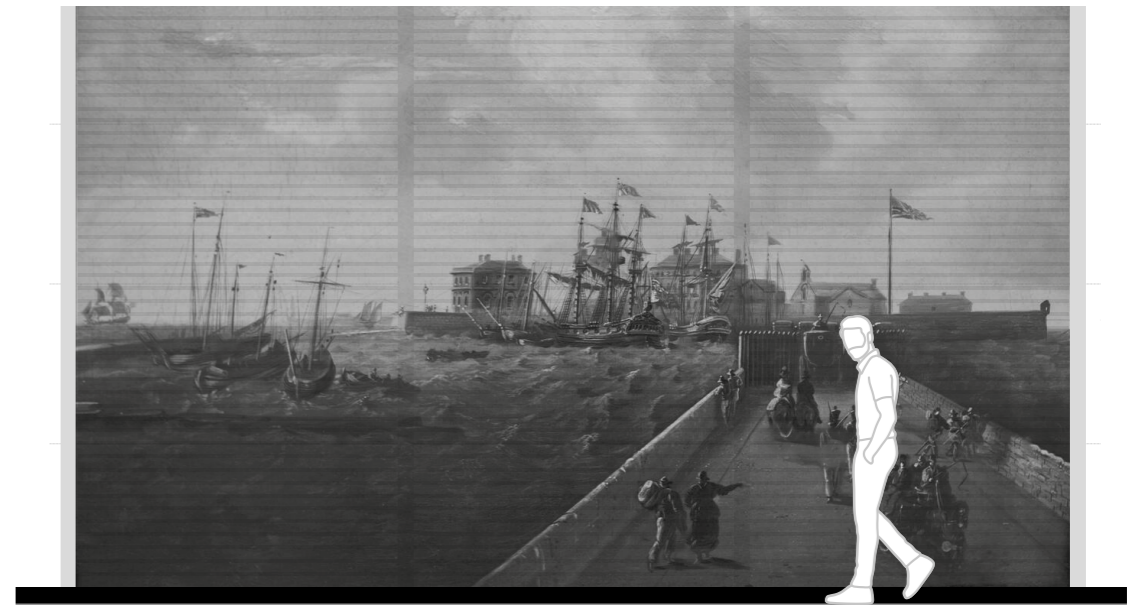


View of graphic image panel incorporated into bench seating element opposite the proposed Maritime Village



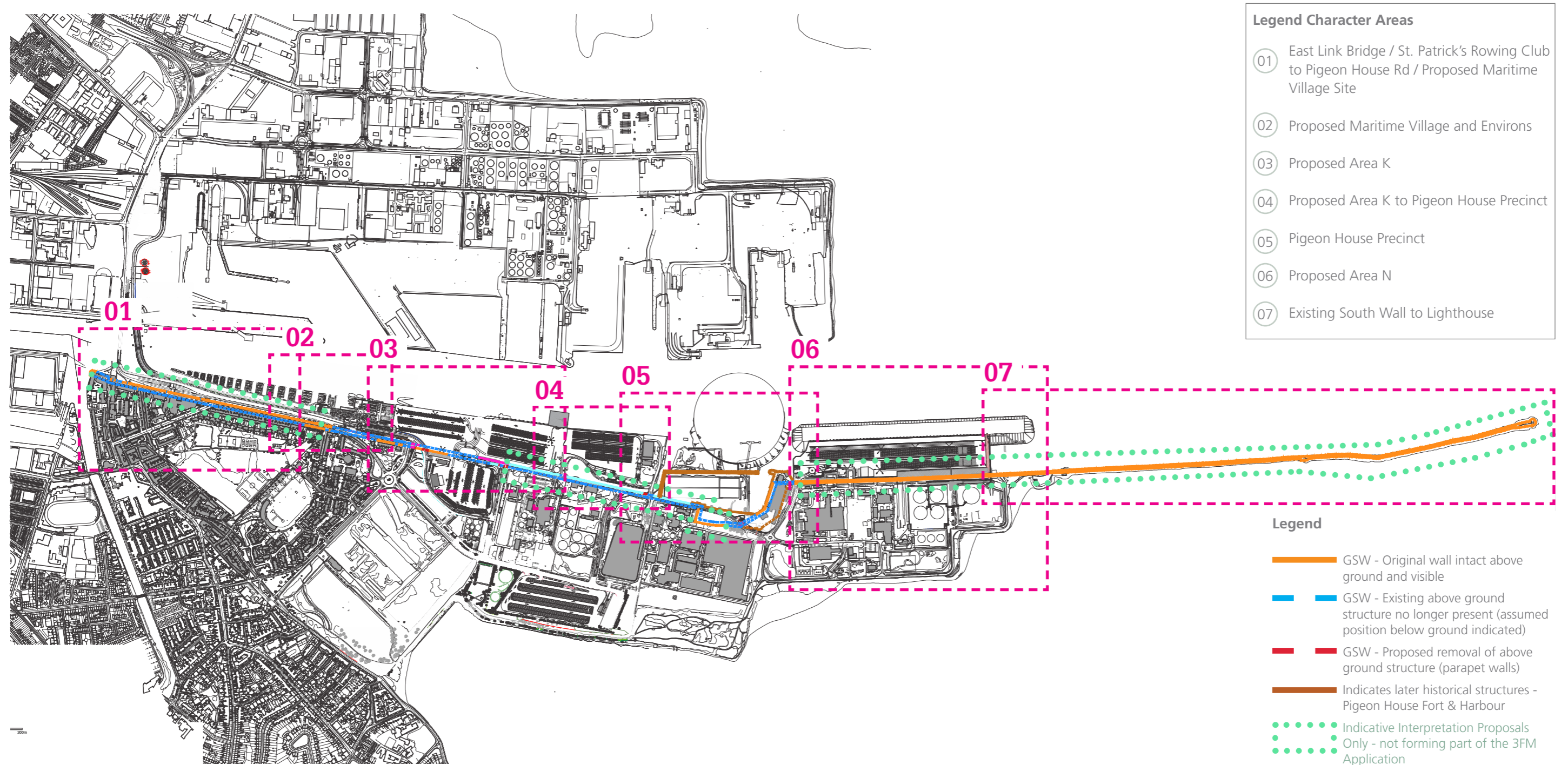
Reference Image of historical image transposed onto a transparent perforated metal screen, by Graepels along the Wild Atlantic Way

View of graphic image on acoustic wall, depicting View of the Pigeon House and South Wall, Dublin, Ireland. Painted by William Sadler, 1810. National Gallery of Ireland





Proposed Masterplan



- Legend Character Areas**
- 01 East Link Bridge / St. Patrick's Rowing Club to Pigeon House Rd / Proposed Maritime Village Site
  - 02 Proposed Maritime Village and Environs
  - 03 Proposed Area K
  - 04 Proposed Area K to Pigeon House Precinct
  - 05 Pigeon House Precinct
  - 06 Proposed Area N
  - 07 Existing South Wall to Lighthouse

- Legend**
- GSW - Original wall intact above ground and visible
  - GSW - Existing above ground structure no longer present (assumed position below ground indicated)
  - GSW - Proposed removal of above ground structure (parapet walls)
  - Indicates later historical structures - Pigeon House Fort & Harbour
  - Indicative Interpretation Proposals Only - not forming part of the 3FM Application

▲ Proposed Masterplan of the Great South Wall

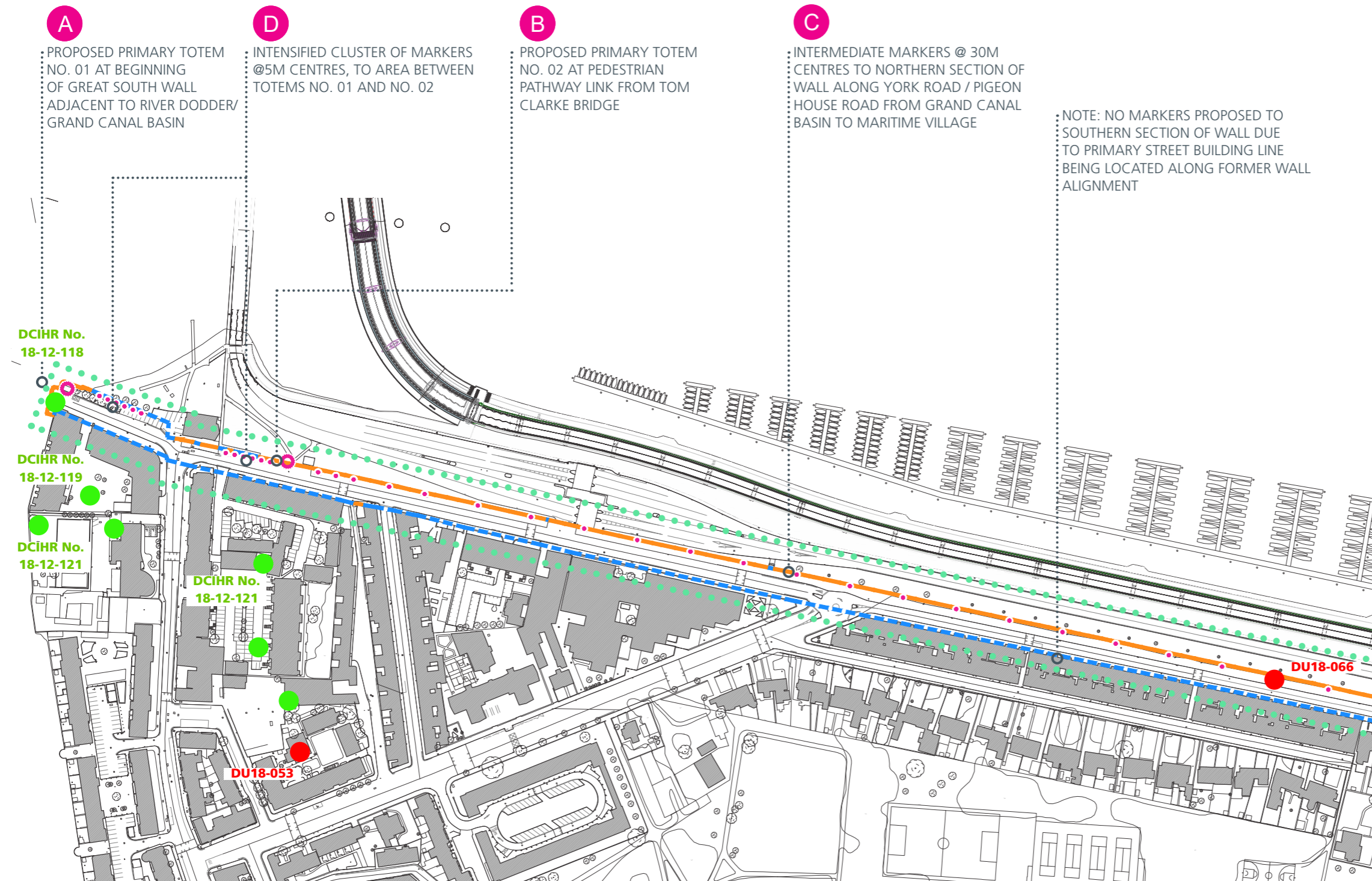
To be read in conjunction with RPS Drawings (S26-HER-XX-DR-HE 100-1000 to 1006) for detailed Proposed Interventions of the Great South Wall



Section 03



Proposed Masterplan - Area 1 of Great South Wall: East Link Bridge / St. Patrick's Rowing Club to Pigeon House Rd / Proposed Maritime Village Site



**A**

PROPOSED PRIMARY TOTEM NO. 01 AT BEGINNING OF GREAT SOUTH WALL ADJACENT TO RIVER DODDER/ GRAND CANAL BASIN

**D**

INTENSIFIED CLUSTER OF MARKERS @5M CENTRES, TO AREA BETWEEN TOTEMS NO. 01 AND NO. 02

**B**

PROPOSED PRIMARY TOTEM NO. 02 AT PEDESTRIAN PATHWAY LINK FROM TOM CLARKE BRIDGE

**C**

INTERMEDIATE MARKERS @ 30M CENTRES TO NORTHERN SECTION OF WALL ALONG YORK ROAD / PIGEON HOUSE ROAD FROM GRAND CANAL BASIN TO MARITIME VILLAGE

NOTE: NO MARKERS PROPOSED TO SOUTHERN SECTION OF WALL DUE TO PRIMARY STREET BUILDING LINE BEING LOCATED ALONG FORMER WALL ALIGNMENT

Indicative Interpretation Proposals Only - not forming part of the 3FM Application



Keyplan showing extent of the Great South Wall

York Road / Pigeon House Road to Proposed Maritime village

This area marks the beginning of the Great South Wall, constructed in 1786. Today, York Road and Pigeon House Road trace the route of the original wall. The northern parapet of the wall remains largely intact and in good condition, while much of the southern parapet has been lost, with the primary street building line following its former alignment.

The start of the wall is located near the mouth of the River Dodder as it enters the Liffey. Currently, this location is a cul-de-sac with limited foot traffic. To highlight the significance of the Great South Wall's beginning, it is proposed to place two primary totems in this area. The first totem will be situated at the actual start of the wall, and the second totem will be placed near the pedestrian pathway linking York Road to the Tom Clarke Bridge, which receives more significant foot traffic. St. Patrick's Rowing Club is located between these two points. To create a new character area, the two totems will be connected by an intensified cluster of intermediate markers. After this cluster, the intermediate markers will gradually decrease in frequency, with a spacing of 30 metres apart until the next focal point at the proposed Maritime Village.

To be read in conjunction with RPS Drawings (S26-HER-XX-DR-HE 100-1000 to 1006)

Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage (NIAH)
- Dublin City Industrial Heritage Record (DCIHR)
- Record of Protected Structures (RPS)
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface



Proposed Area 1 of the Great South Wall

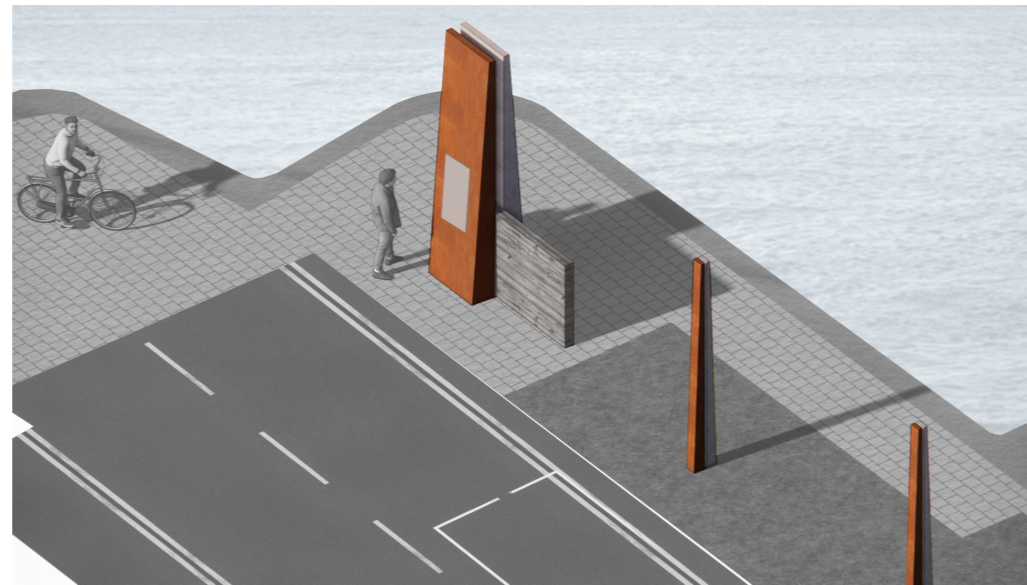
Section 03



## Sections - Area 1 of Great South Wall: East Link Bridge / St. Patrick's Rowing Club to Pigeon House Rd / Proposed Maritime Village Site

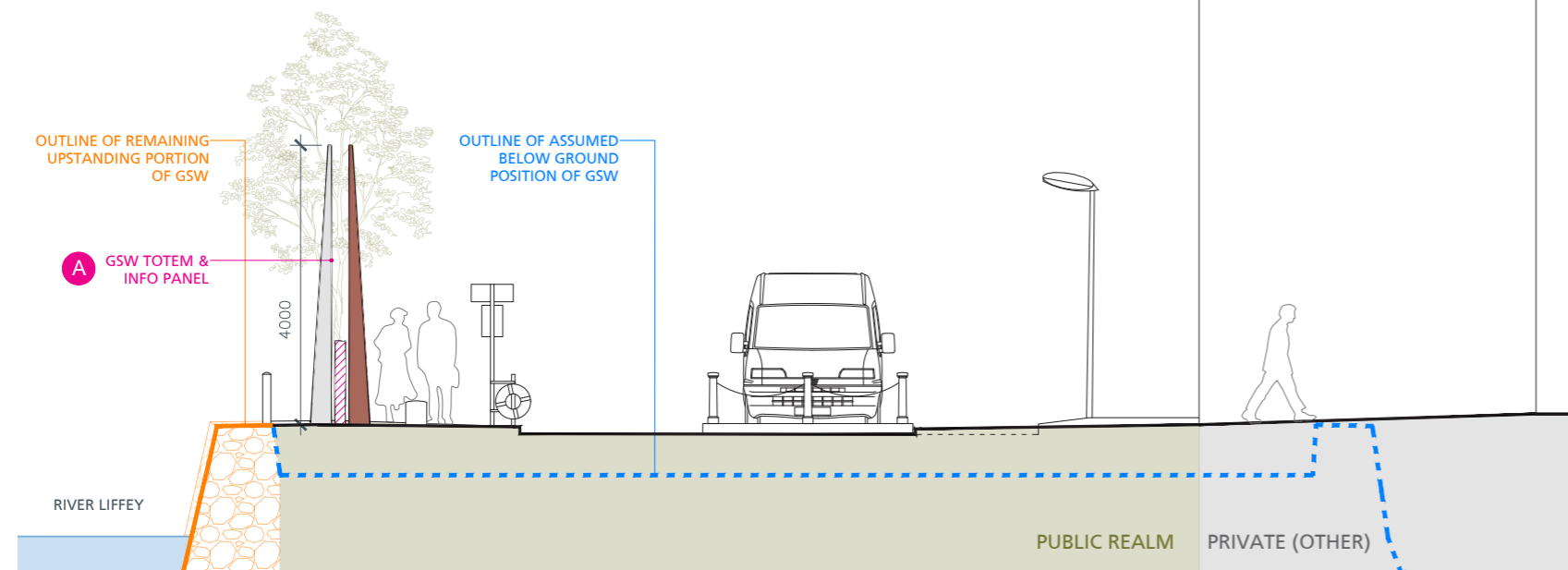
### Introducing the Great South Wall

This area in Ringsend marks the start of the Great South Wall (GSW) when it was built in 1786. This spot, which now marks the end of Thorncastle street, shares views towards the neighbouring quays across the River Liffey. Although, public footpaths and greens are in close proximity to the site, it is located at the sheltered end of Thorncastle street and surrounded by residential apartment blocks making the wall out of view. An interpretative totem of significant scale with an information panel would benefit this location - acting as an informative piece about the monumental history of the GSW. This would be followed by a cluster of poles installed with lighting elements which mark its assumed position below ground.



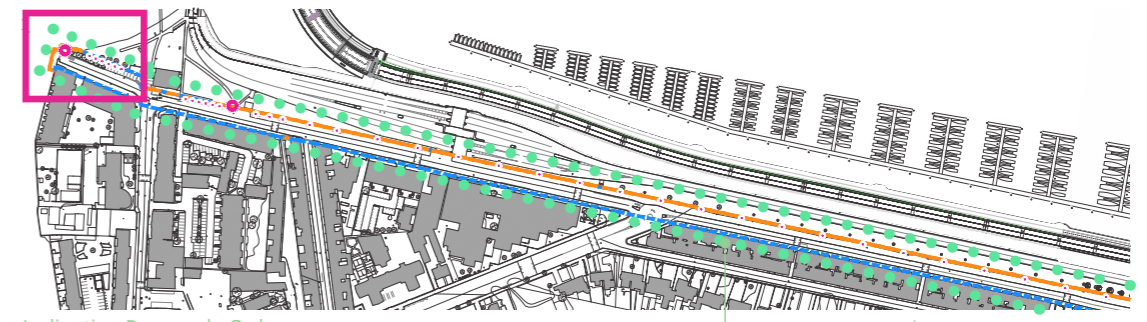
▲ Proposed GSW Totem & Pole Interpretation Elements Types A & D, courtesy of BRIGHT

As highlighted in the key plan, this area's proposals are indicative only and not within Dublin Port ownership. It does not form part of the 3FM application and would require separate planning consent in the future.



▲ Section 01-01  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Indicative Proposals Only  
(Area not within Dublin Port ownership)

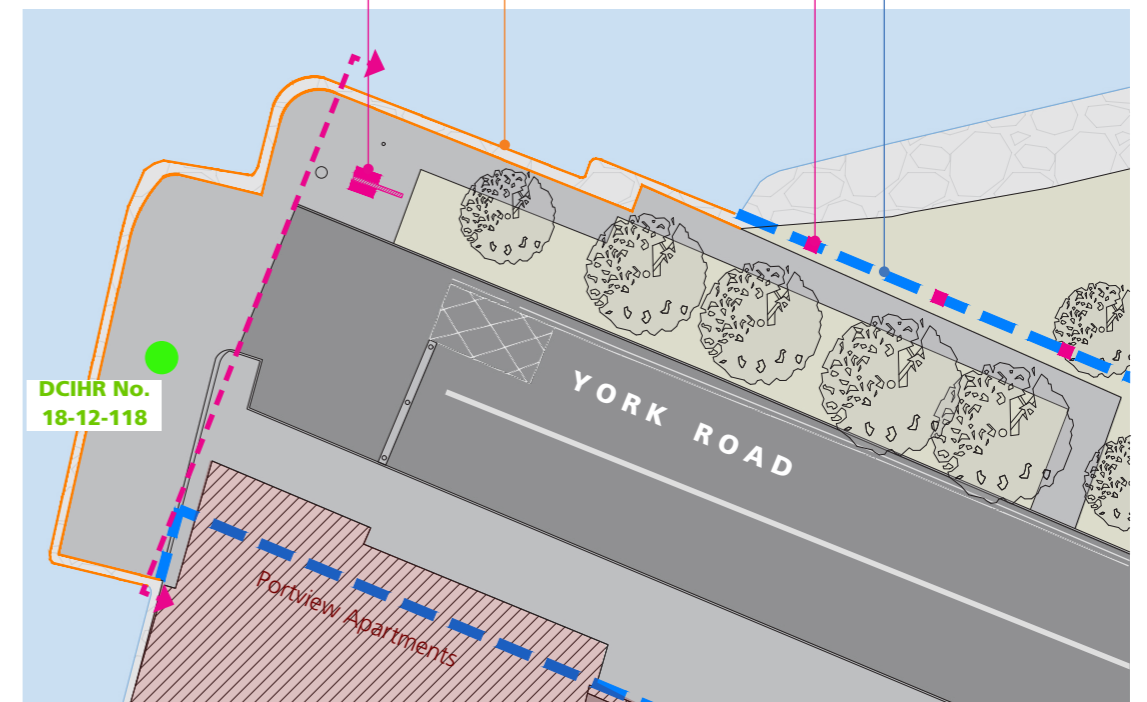
▲ Area 1 Key Plan

**A** GSW TOTEM ON NEW EXTENDED HARD SURFACE

**D** CLUSTER OF ILLUMINATING POLES MARKING THE GSW

OUTLINE OF REMAINING GSW AS QUAY WALLS

OUTLINE OF ASSUMED BELOW GROUND POSITION OF GSW



### Section 03

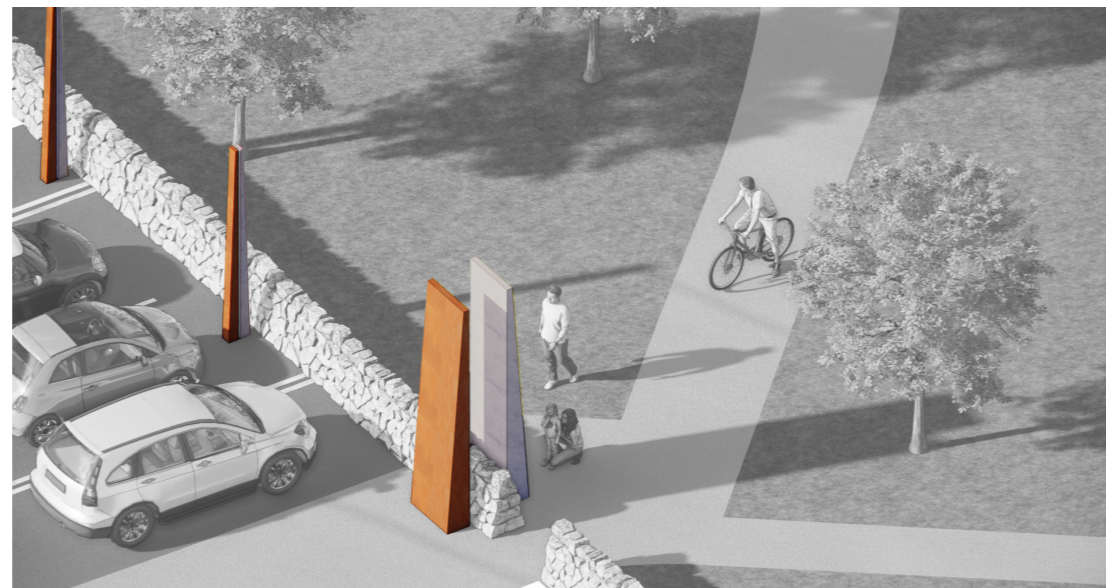


# Sections - Area 1 of Great South Wall: East Link Bridge / St. Patrick's Rowing Club to Pigeon House Rd / Proposed Maritime Village Site

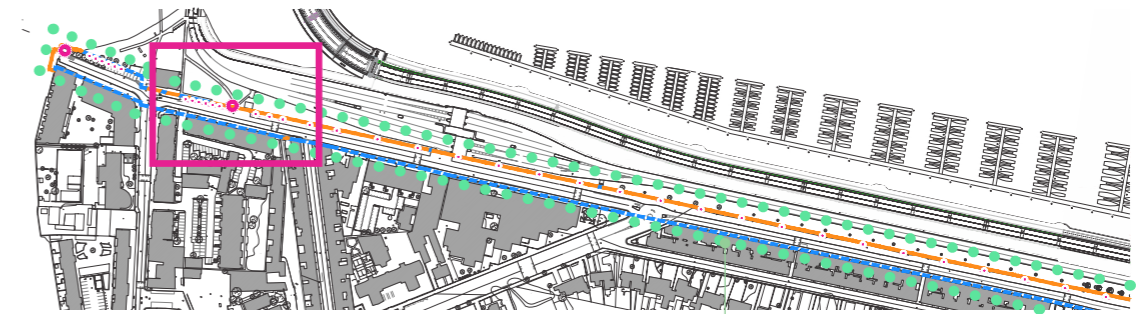
## York Rd.

York Road, just after St. Patrick's Rowing Club, is comprised with mainly residential apartment blocks which were built in line with the assumed below ground position of GSW. As seen in this section and key plan, this location is particularly busy with street parking - situated opposite to the residential units where the GSW remains above ground. Behind the GSW are merging footpaths from the public green, all leading towards a pedestrian access through/from the street parking. A GSW totem is recommended to be placed here to catch the pedestrian traffic that comes through at this point.

As highlighted in the key plan, this area's proposals are indicative only and not within Dublin Port ownership. It does not form part of the 3FM application and would require separate planning consent in the future.



▲ Proposed GSW Totem & Pole Interpretation Elements Types B & D, courtesy of BRIGHT



Indicative Proposals Only  
(Area not within Dublin Port ownership)

▲ Area 1 Key Plan

PRIVATE RESIDENTIAL APARTMENT BLOCKS

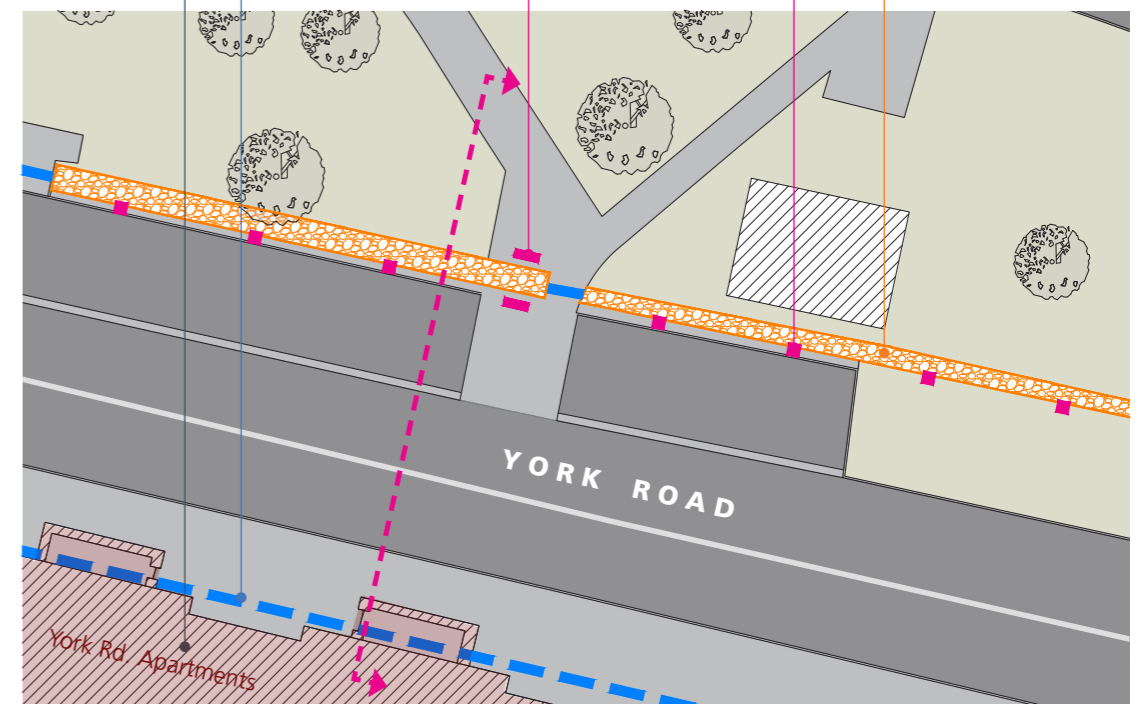
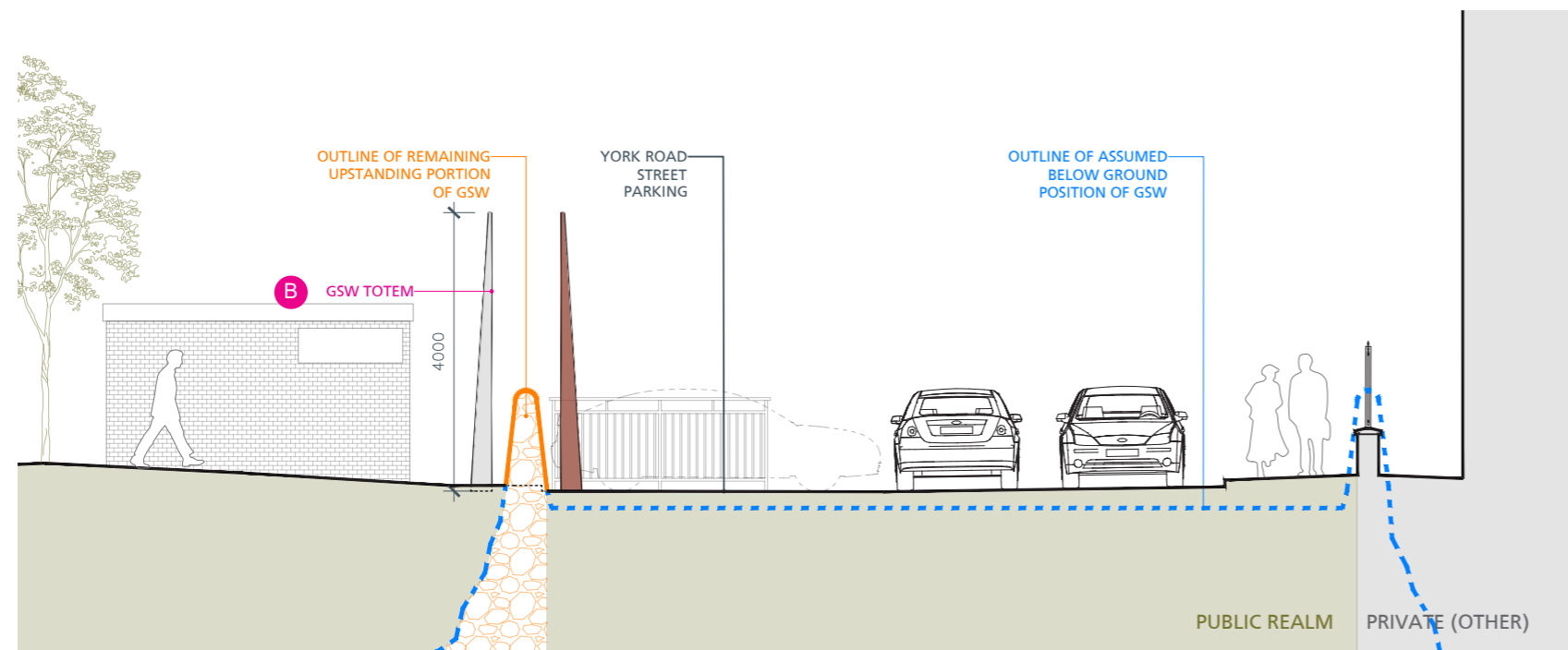
**B** GSW TOTEM ON NEW EXTENDED HARD SURFACE

**D** CLUSTER OF ILLUMINATING POLES MARKING THE GSW

OUTLINE OF ASSUMED BELOW GROUND POSITION OF GSW

Note: Street Parking reduced to suit GSW marker placement

OUTLINE OF REMAINING UPSTANDING POSITION OF GSW



▲ Section 02-02  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW

- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Section 03



## Sections - Area 1 of Great South Wall: East Link Bridge / St. Patrick's Rowing Club to Pigeon House Rd / Proposed Maritime Village Site

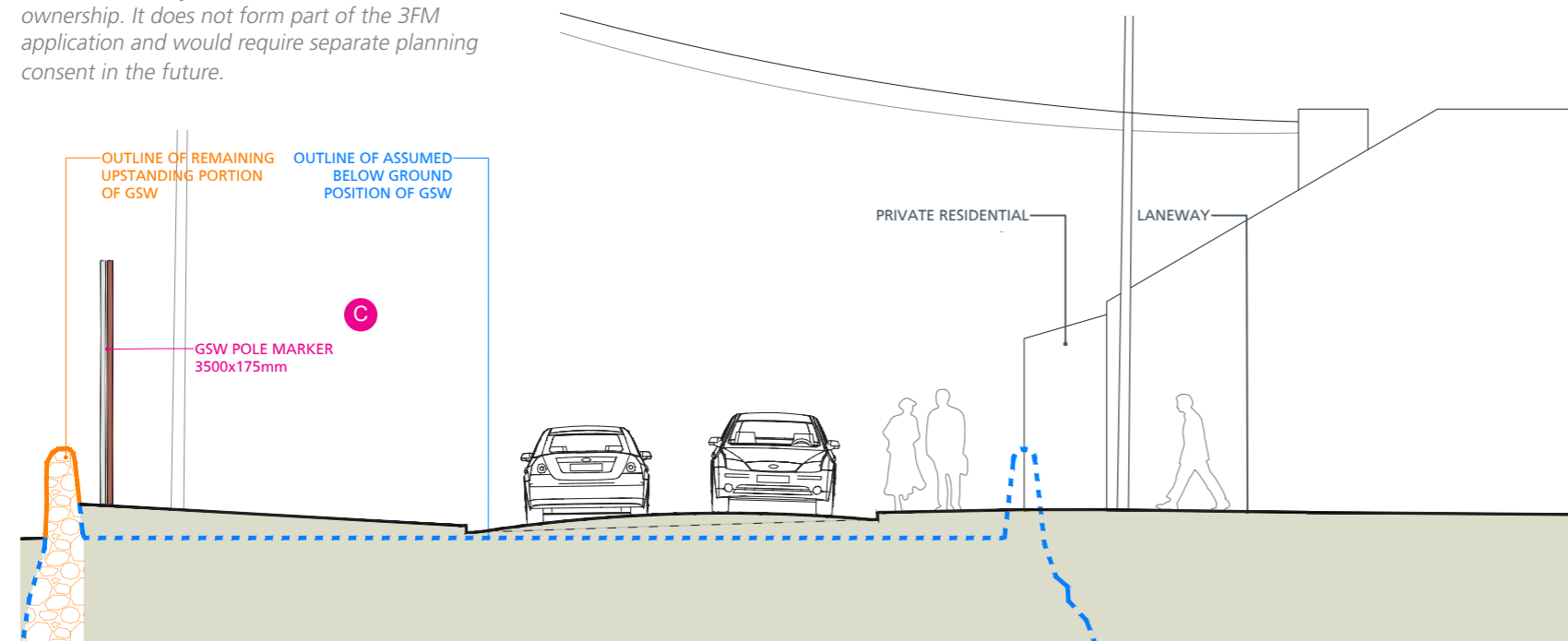
### Pigeon House Rd. - Cottages & Laneways

This section of Pigeon House Rd. is lined with residential cottages to one side. Adjacent to these cottages is a green verge where a remainder of the GSW stands. The foot paths in front of these houses cannot cater for poles to stand at each side of the street. The street is also busied with electrical poles and street lighting. To respect its existing context, proposed GSW pole markers are proposed at 30m intervals along the green verge adjacent to the existing GSW northern parapet at intervals of 30m. By placing the vertical markers consistently along existing sections of wall, the public will begin to make the association between the wall and the markers, which will help with understanding the meaning of the markers in areas where when the wall is no longer present.



▲ Pigeon House Rd. street view towards cottages with proposed intermediate markers Type C at 30m centres, courtesy of BRIGHT

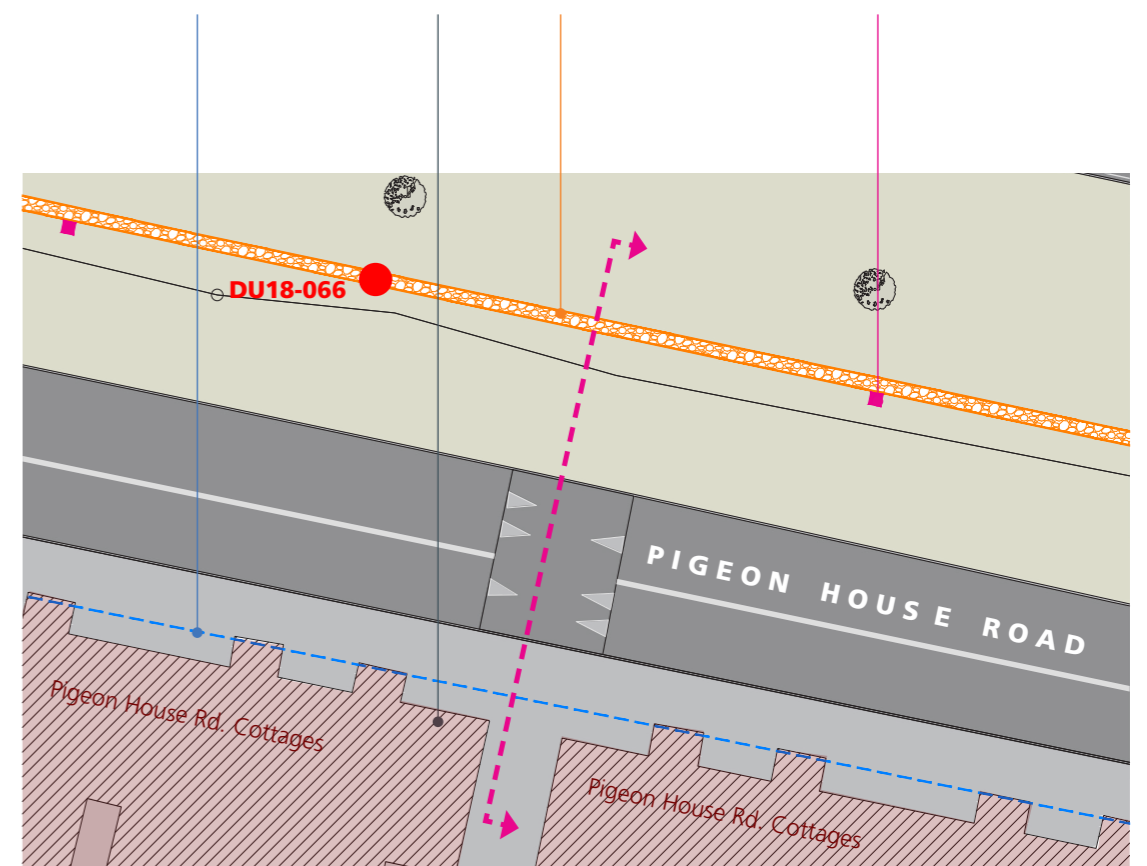
As highlighted in the key plan, this area's proposals are indicative only and not within Dublin Port ownership. It does not form part of the 3FM application and would require separate planning consent in the future.



Indicative Proposals Only  
(Area not within Dublin Port ownership)

▲ Area 1 Key Plan

- OUTLINE OF ASSUMED BELOW GROUND POSITION OF GSW
- PRIVATE RESIDENTIAL COTTAGES & PUBLIC LANEWAYS
- OUTLINE OF REMAINING UPSTANDING POSITION OF GSW
- POLES MARKING THE GSW WITH GREATER DISTANCE IN BETWEEN



▲ Section 2B-2B  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- ↑ Section Line Through Site
- █ Proposed Removal of GSW

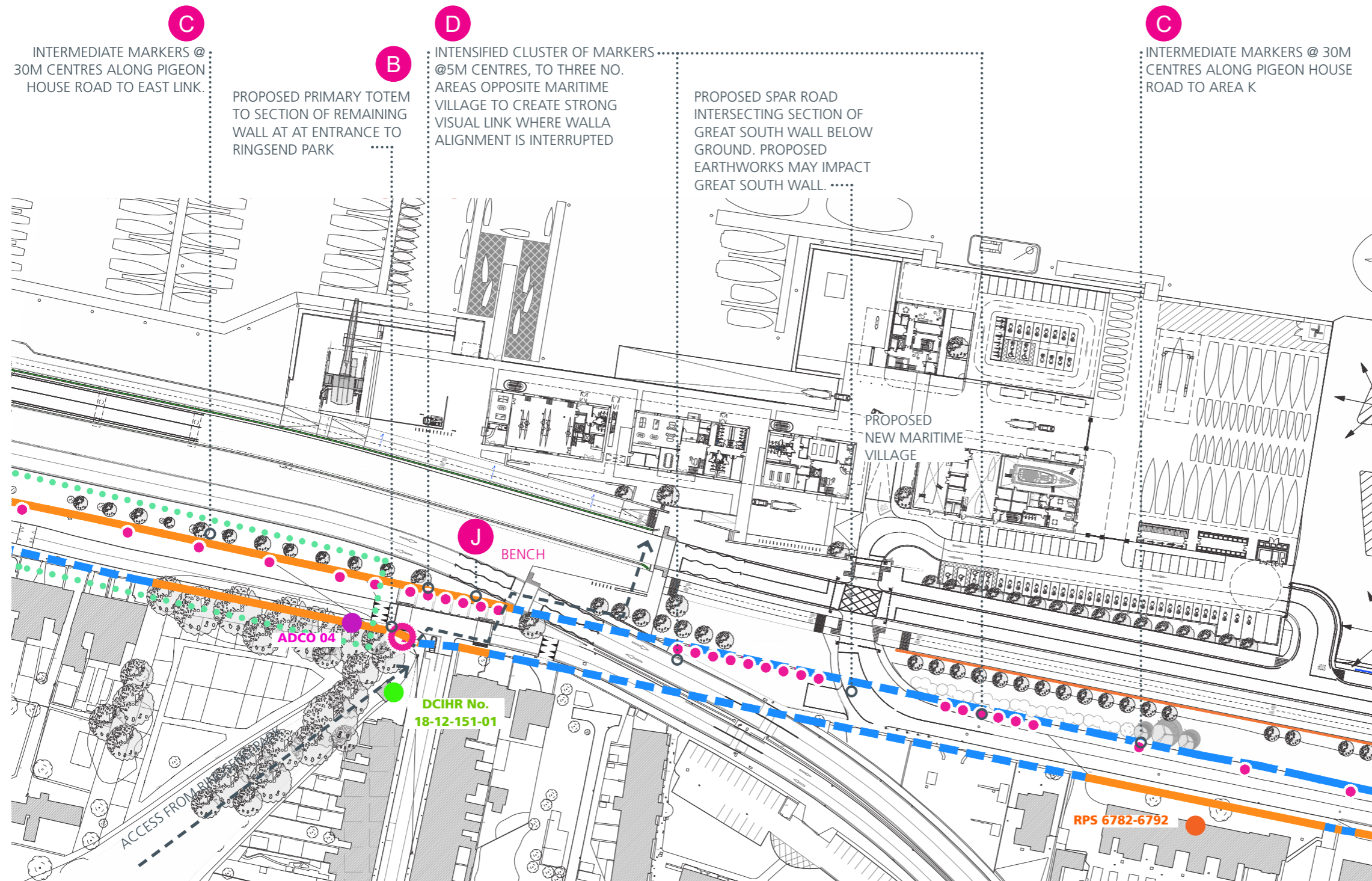
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



### Section 03



Proposed Masterplan - Area 2 of Great South Wall: Proposed Maritime Village & Environs



Indicative Interpretation Proposals Only - not forming part of the 3FM Application



Keyplan showing extent of the Great South Wall

Pigeon House Rd. - Proposed Maritime Village

The area surrounding the new Maritime Village is poised for significant transformation due to the construction of the new SPAR Road, an active travel route, and new vehicular and pedestrian access routes. The existing alignment of the Great South Wall is already significantly interrupted around this location due to the earlier construction of the East Link Road.

The development of the Maritime Village will establish a prominent new focal point in the area, enhancing connectivity for the local community and providing an opportunity to further highlight the Great South Wall.

To address the visual disruptions caused by road infrastructure, existing & planned, proposals for this area aim to create legible visual connections marking the wall's alignment. At three key points, as indicated adjacent, it is proposed to intensify the spacing of the vertical markers. This strategy will establish a strong visual character, allowing the wall's alignment to be clearly discerned from one point to the next. Beyond this area, the markers will transition to a more comfortable spacing of 30 metres along the remainder of Pigeon House Road, maintaining continuity and guiding visitors through the historical landscape.

To be read in conjunction with RPS Drawings (S26-HER-XX-DR-HE 100-1000 to 1006) & ACTIVE TRAVEL ROUTE DRAWINGS (at Proposed Maritime Village) Refer to Darmody Architecture Drawing: CP1901-DA-00-XX-DR-A-PA-001-B & CP1901-DA-00-XX-DR-A-PA-200-B

- Refer to **3FM EAIR Chapter 16 Appendices**
- National Inventory of Architectural Heritage (NIAH)
  - Dublin City Industrial Heritage Record (DCIHR)
  - Record of Protected Structures (RPS)
  - ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface



Section 03



# Proposed Masterplan - Area 2 of Great South Wall: Proposed Maritime Village & Environs

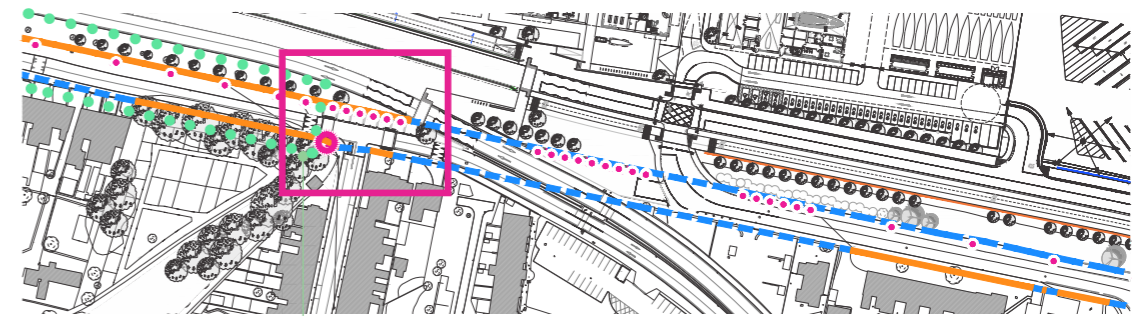
## Pigeon House Rd., Ringsend Park & Proposed Maritime Village

The GSW continues through this nodal point between the Pigeon House Rd., Ringsend Park and the new Maritime Village. Evident in the key maps, the GSW remains above ground and disappears as it meets the new The location Active Travel from the Maritime Village side, and the layout of the new pedestrian crossing has been designed to take account of and respect the existing portion of the GSW, ensuring there is no impact upon it.

Currently, the GSW wall opens up an access route through Ringsend Park, making it a strategic point for a proposed informative, interpretative totem. On the other side, the poles are continued along Pigeon House Rd. with an intensified spacing, shaping the public realm experience along the Maritime Village during day and night. An additional graphic panel is also incorporated into the bench furniture at this location which will depict a historical image of the GSW, further adding to the richness of the proposals.



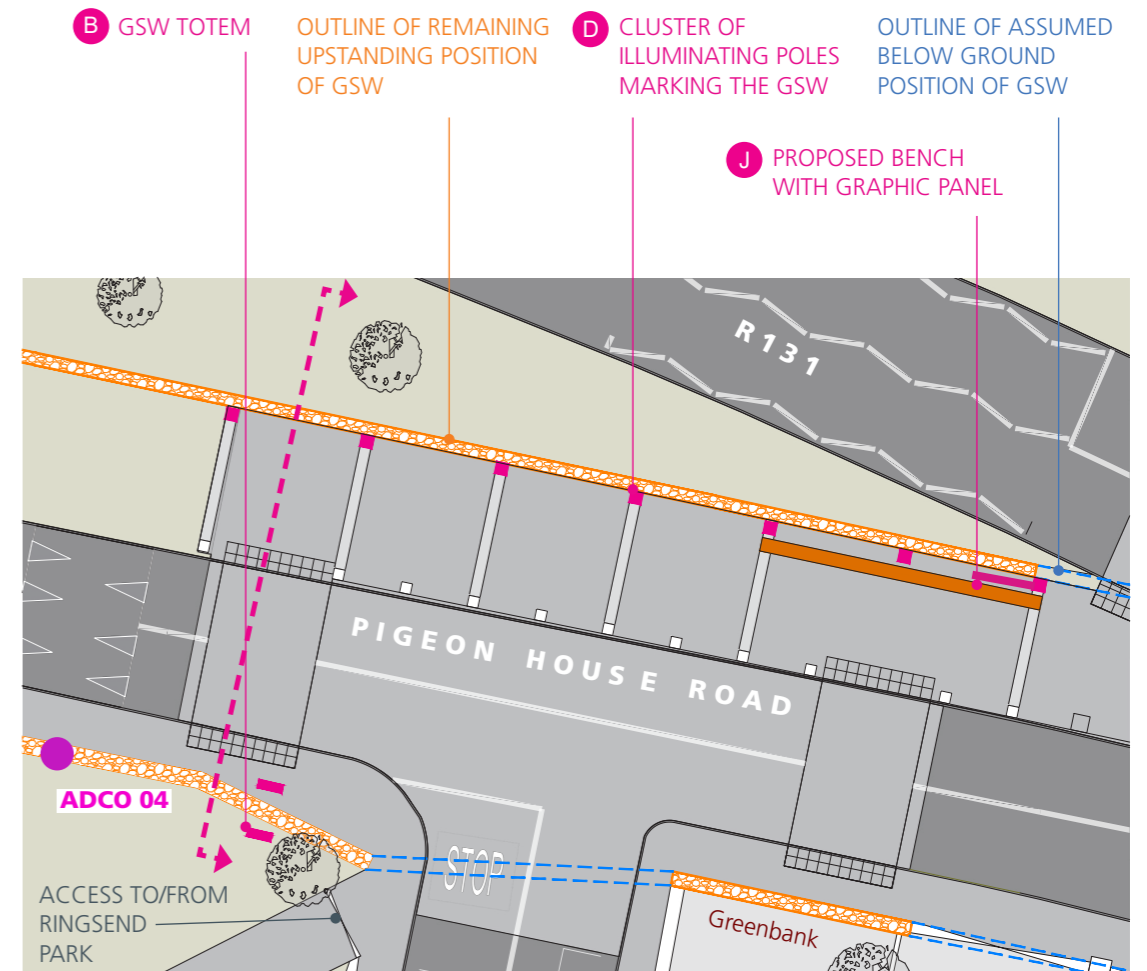
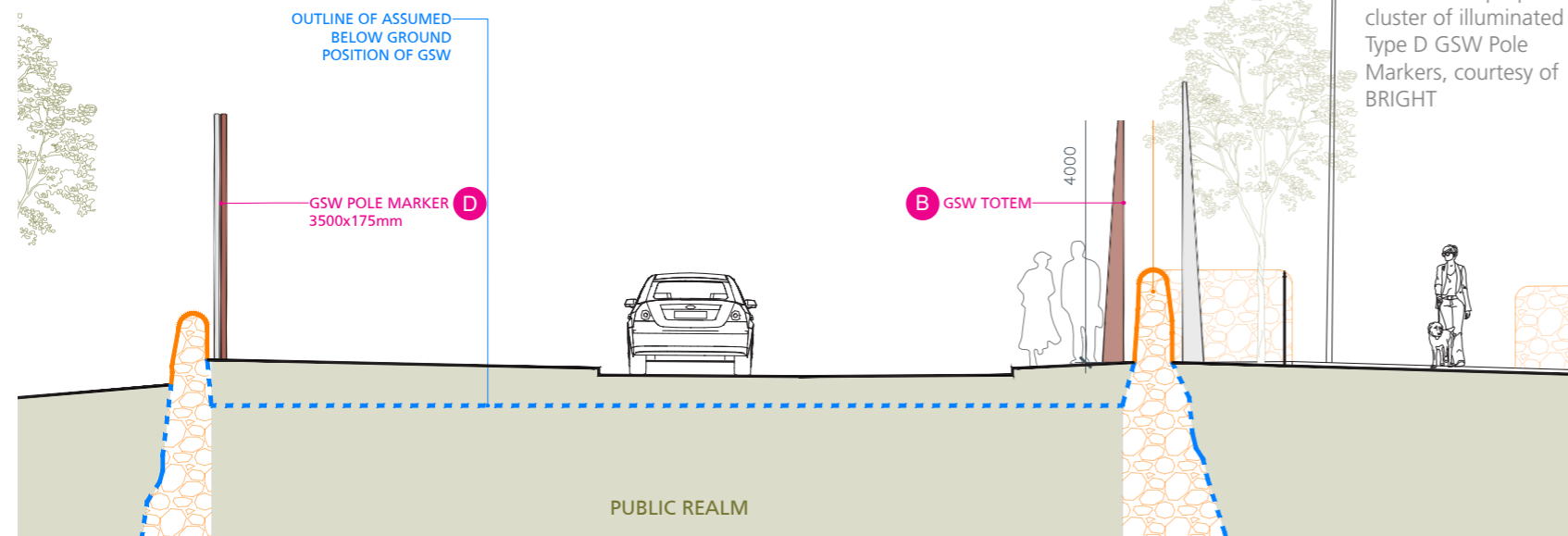
To be read in conjunction with ACTIVE TRAVEL ROUTE DRAWINGS (at Proposed Maritime Village) Refer to Darmody Drawings: CP1901-DA-00-XX-DR-A-PA-001-B & CP1901-DA-00-XX-DR-A-PA-200-B



Indicative Proposals Only (Area not within Dublin Port ownership)

Area 2 Key Plan

Nighttime view towards the proposed Maritime Village depicts an engaging atmosphere as a result of the proposed cluster of illuminated Type D GSW Pole Markers, courtesy of BRIGHT



Section 03-03  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



## Section 03



Proposed Masterplan - Area 2 of Great South Wall: Proposed Maritime Village & Environs

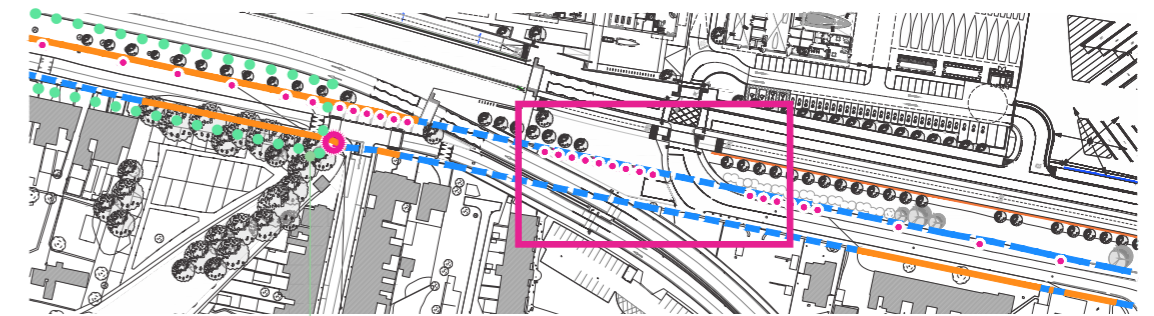
**Pigeon House Rd., Ringsend Park & Proposed Maritime Village**

The GSW disappears significantly beneath existing and proposed roads in this section. However, the new public realm between these roads along with the new Active Travel provide a greater opportunity to create an impactful way of marking the GSW below ground. The line of tightly clustered poles along green verges direct our eyes towards the direction of the wall through its contemporary interpretation - creating a strong visual connection. These poles will be illuminated at night to further help create a strong character. Where a new vehicular entrance into the proposed Maritime Village is created, the alignment of the wall is further marked across the carriageway with a line of metal stud markers. Together, these various interpretation proposals around the new maritime village area will ensure that the legibility of the wall is strengthened and celebrated.

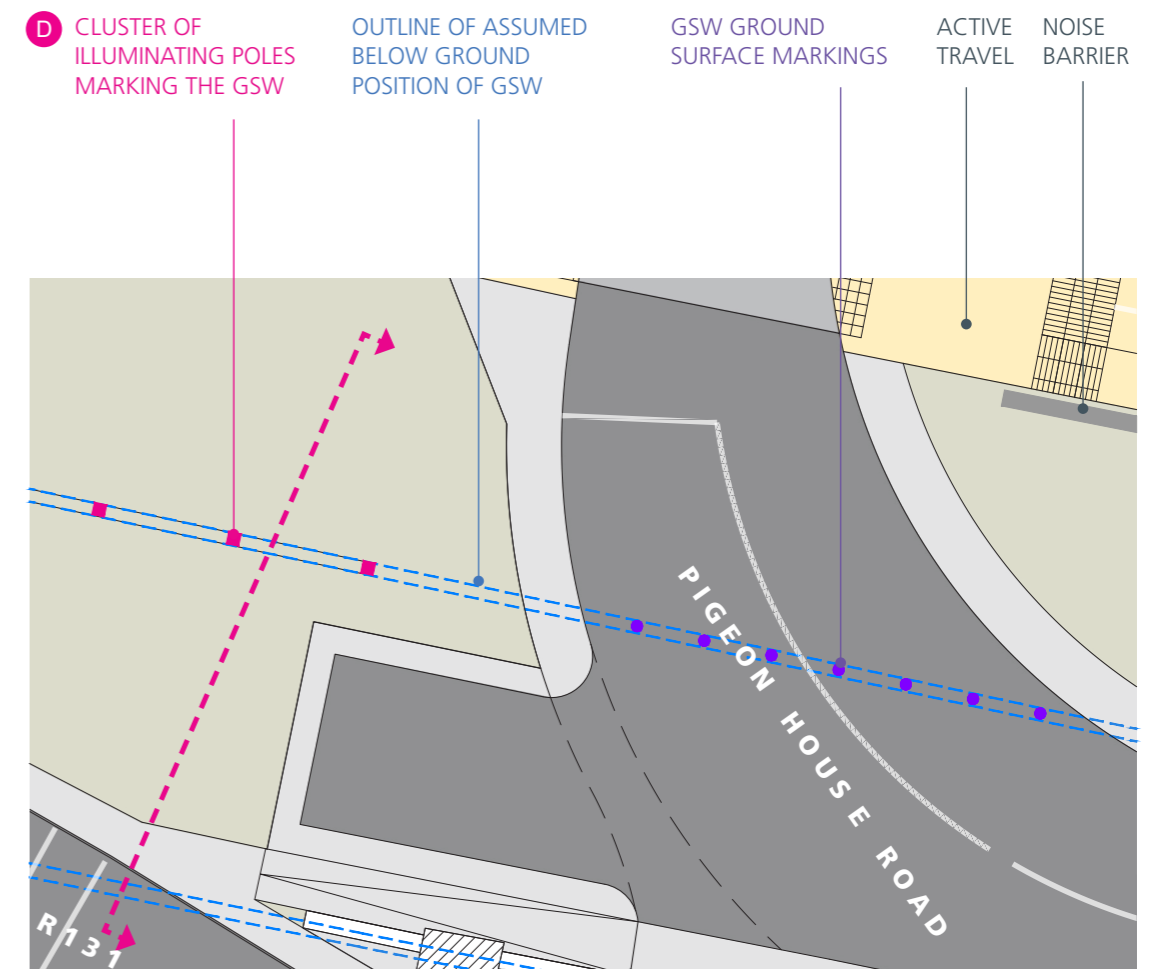
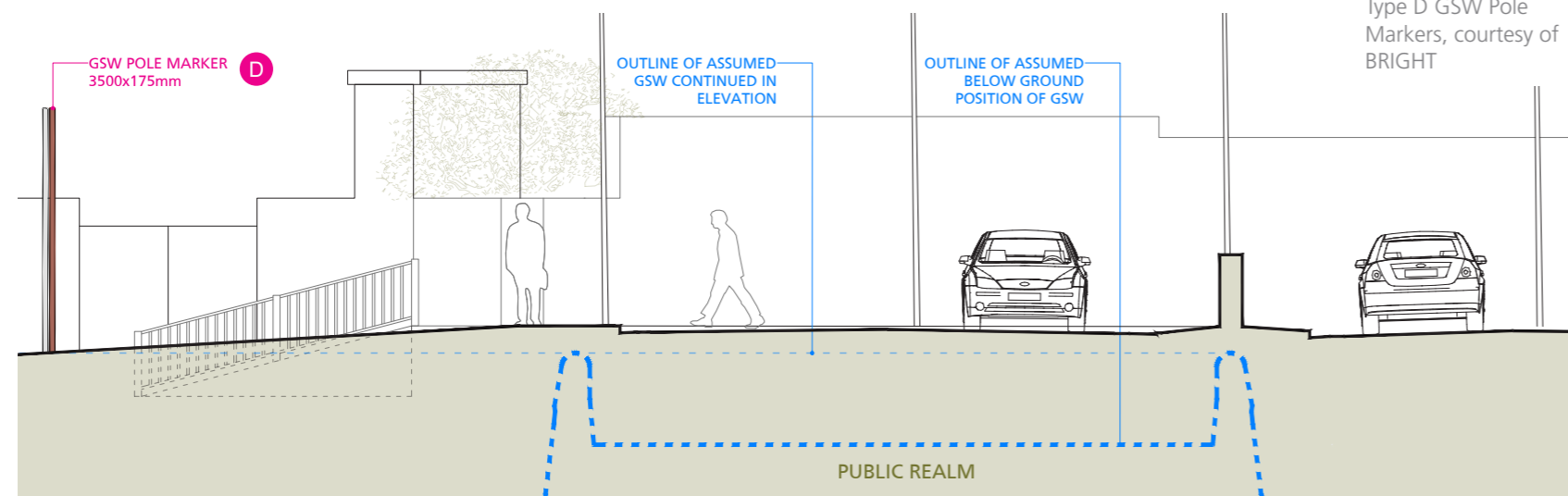


To be read in conjunction with ACTIVE TRAVEL ROUTE DRAWINGS (at Proposed Maritime Village) Refer to Darmody Drawings: CP1901-DA-00-XX-DR-A-PA-001-B & CP1901-DA-00-XX-DR-A-PA-200-B

▲ Nighttime view towards the proposed Maritime Village depicts an engaging atmosphere as a result of the proposed cluster of illuminated Type D GSW Pole Markers, courtesy of BRIGHT



Indicative Proposals Only (Area not within Dublin Port ownership) ▲ Area 2 Key Plan



▲ Section 04-04 1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Section 03



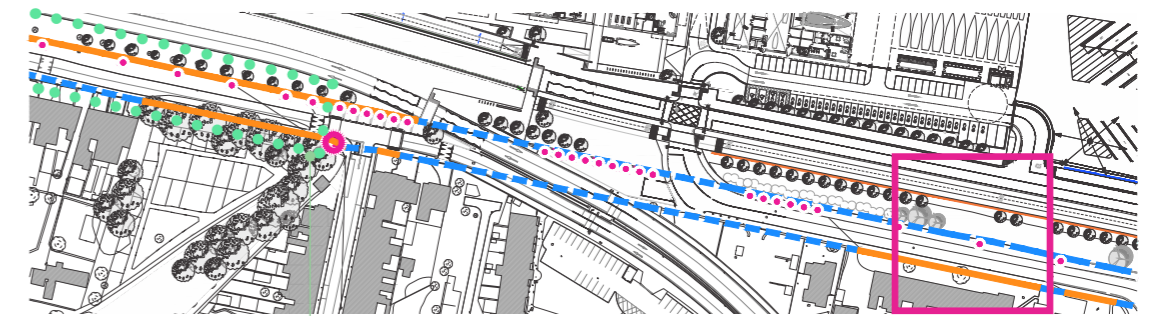
Proposed Masterplan - Area 2 of Great South Wall: Proposed Maritime Village & Environs

**Pigeon House Rd.**

To the right of Pigeon House Rd. is a temporary boat store, a stretch of terraced housing and the 1st Port of Dublin, Ringsend Sea Scouts. While the GSW predominantly remains above ground to the right, it is below ground level to the left. Above this hidden part of the GSW is a low wall dividing the footpath from a green verge filled with trees and shrubbery. Represented in the image to the right is a view down the street where a proposed 4m high noise barrier will stand. This section of Pigeon House Rd. is significant as its route is interrupted at this point by an existing container terminal which cuts it in two, only to re-emerge again on the eastern side of the terminal. To mark the GSW, while respecting its existing surrounding context, proposed GSW pole markers are located to the left side of the road to mark the assumed below ground position of its northern parapet. This approach ensures consistency across the length of the wall, helping to restore the legibility in particular where above ground sections are no longer visible. A proposed graphic panel is also proposed to terminate the view at the end of the road and this is shown in detail on Page 32.

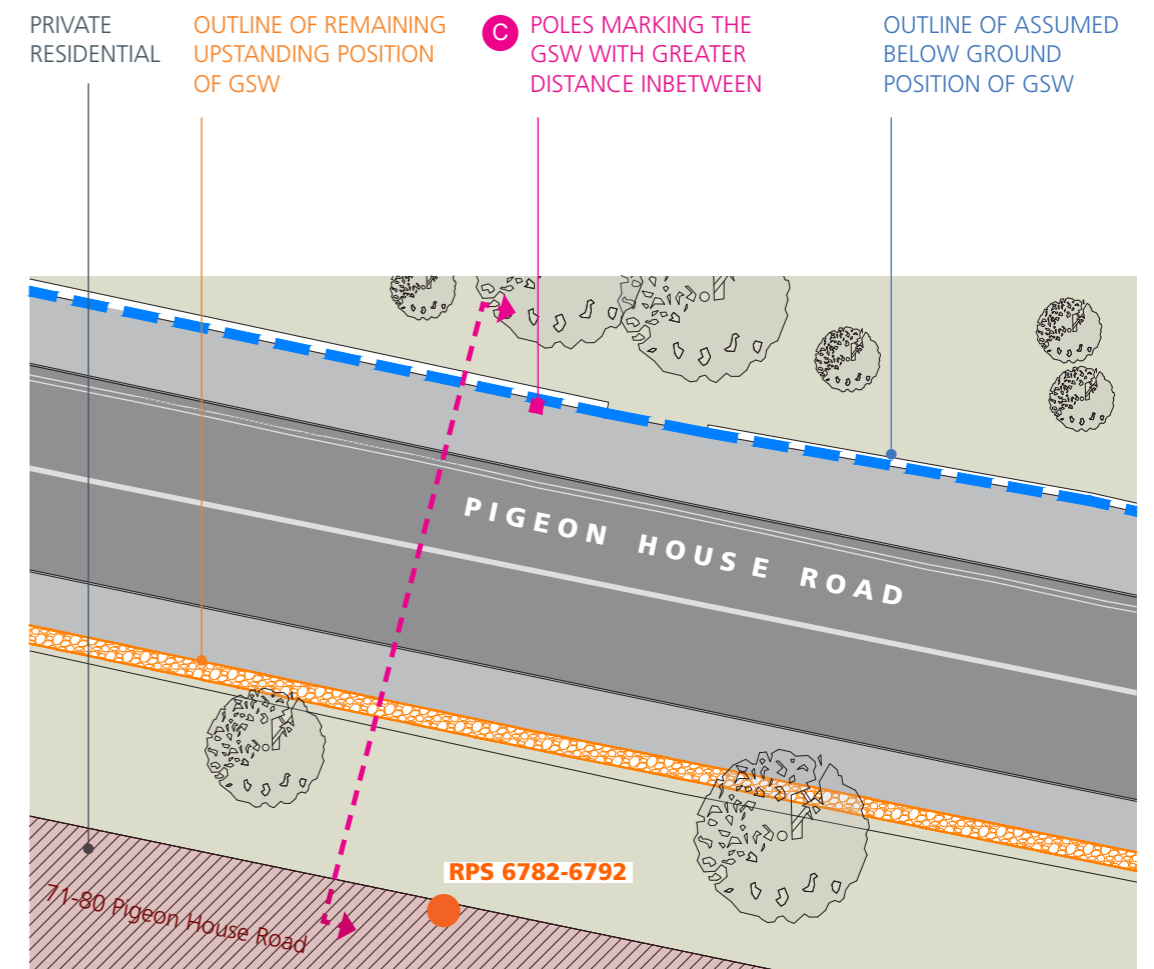
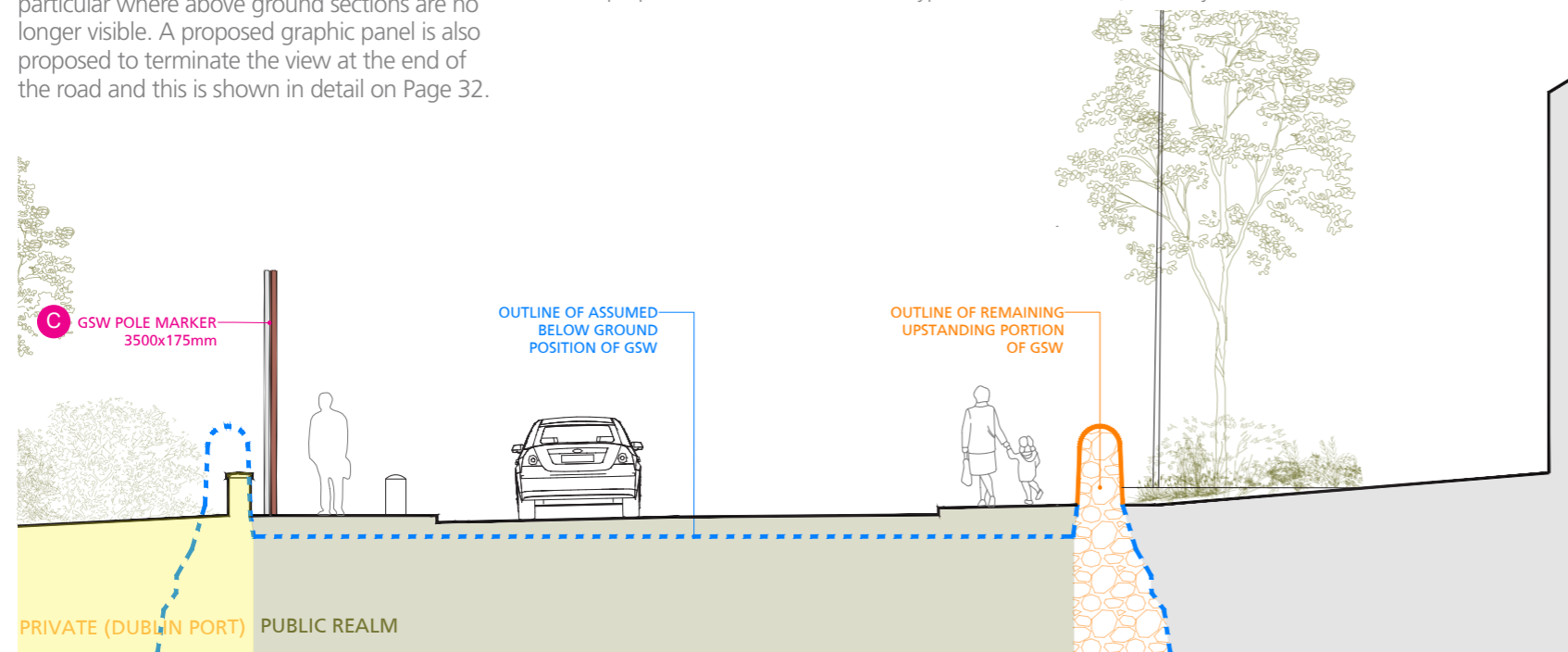


▲ Pigeon House Rd. street view towards proposed 4m high noise barrier with proposed intermediate markers Type C at 30m centres, courtesy of BRIGHT



Indicative Proposals Only  
(Area not within Dublin Port ownership)

▲ Area 2 Key Plan



▲ Section 06-06  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW

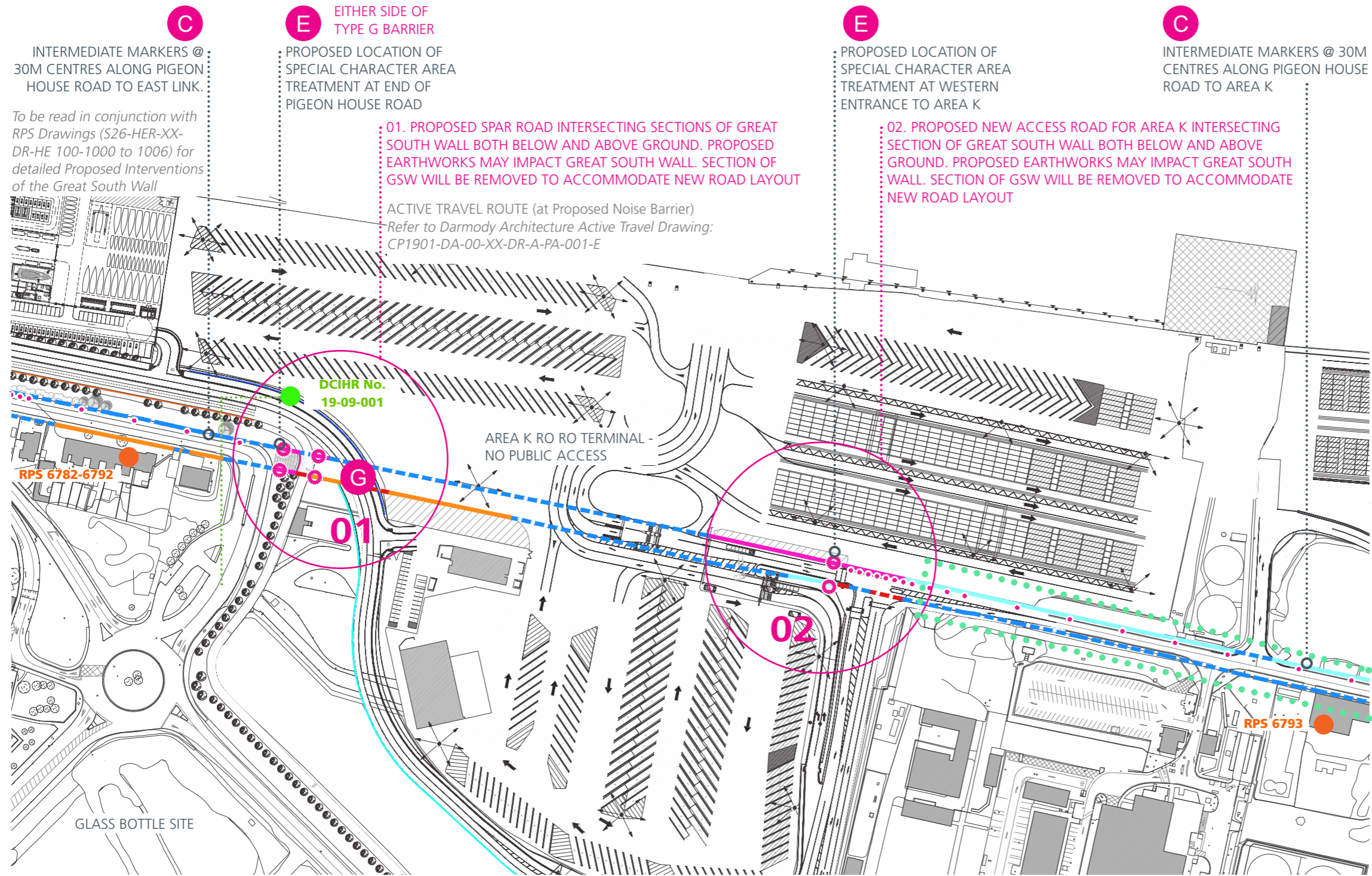
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Section 03



Proposed Masterplan - Area 3 of Great South Wall: Proposed Area K



Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage (NIAH)
- Dublin City Industrial Heritage Record (DCIHR)
- Record of Protected Structures (RPS)
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface



Section 03

●●●●● Indicative Interpretation Proposals Only - not forming part of the 3FM Application



▲ Keyplan showing extent of the Great South Wall

Proposed Area K

Currently, the route of the Great South Wall is disrupted at the end of Pigeon House Road, disappearing into an existing port shipping yard that is inaccessible to the public. This will remain unchanged with the construction of the new Ro-Ro terminal in Area K. The introduction of the new SPAR Road and active travel routes will further disrupt the wall's original alignment, compounded by the addition of 4m high acoustic barriers between the new SPAR Road and Pigeon House Road.

Due to heavy port operational constraints within Area K, placing interpretive markers is not feasible, resulting in a loss of legibility for the wall's route in this area. However, an existing portion of the wall within the yard will be maintained and safeguarded. Regrettably, some smaller portions of the remaining wall will be lost to facilitate the new road infrastructure. To mitigate against the loss, it is proposed to incorporate the salvaged wall material into the design of the interpretive elements as well as rebuilding/repairing the GSW.

Furthermore, to address this significant juncture where the wall's route is lost to the public realm, it is proposed to establish two new marking points at the easternmost and westernmost extremities of Area K. These points will delineate where the wall's route re-enters the public realm on either side, ensuring the continuity of its historical narrative despite the interruptions. The design of these areas is covered in detail on the following pages.



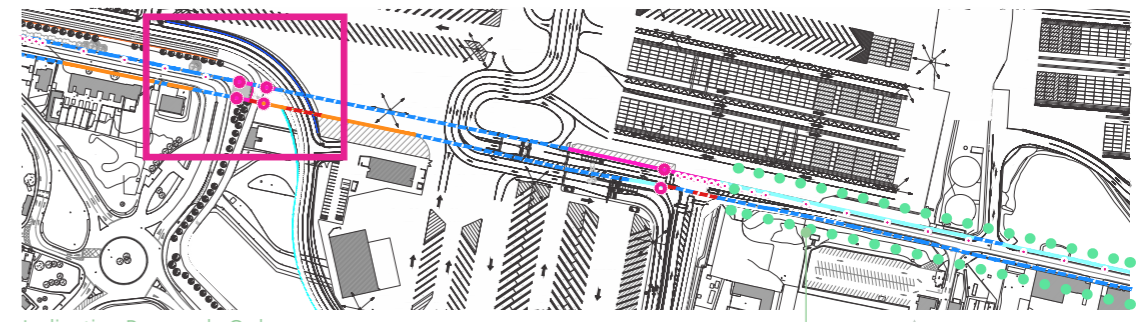
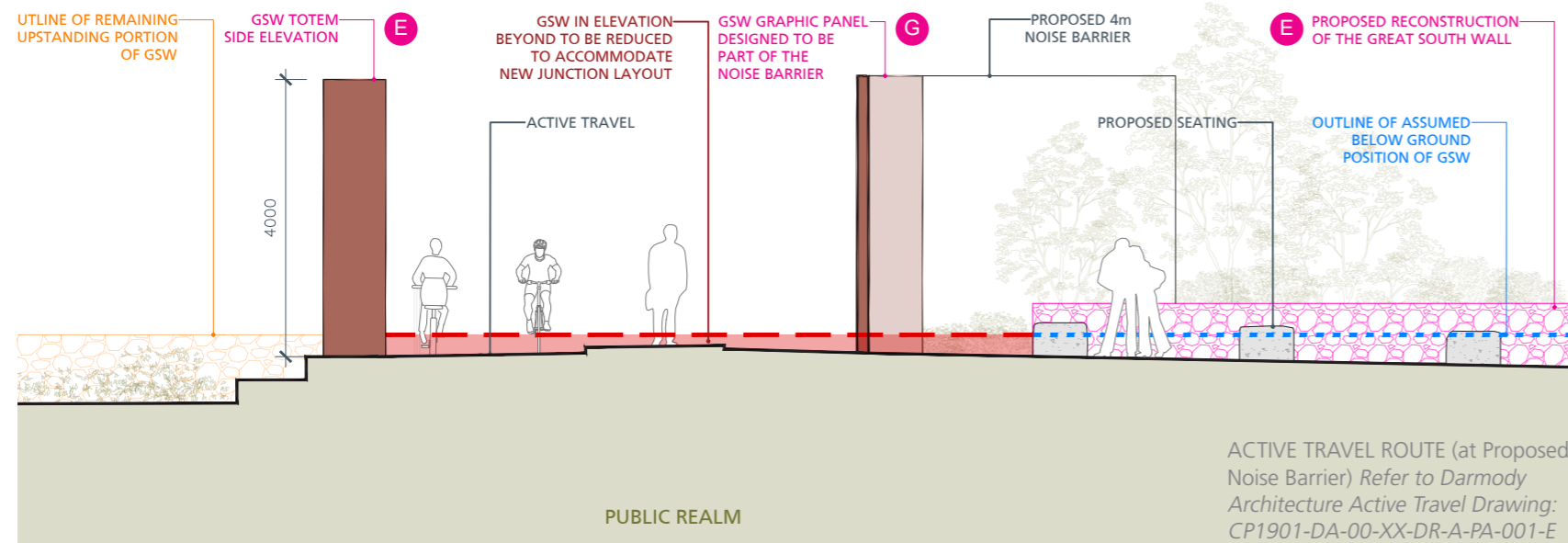
## Sections - Area 3 of Great South Wall: Proposed Area K

### Pigeon House Rd. - SPAR

The main obstacle of this part of the GSW is the proposed 4m noise barrier, resulting from the proposed SPAR, consisting of new roads, pedestrian and cycling routes from the Active Travel. Private shipping yards are pushed back and enclose a significant amount of the GSW (below and above ground). This means that this particular section is the last visual interpretation of the GSW without obstructive elements on site until the next publicly accessed location it reappears. In this area, some portions of the existing upstanding GSW parapet walls will be removed to accommodate the new roads layout. To mitigate against this loss, we are proposing to reduce this demolition to keep some of the existing wall as part of the interpretation, and those removed to be used for the reconstruction of the wall in between the proposed GSW Totems, or as part of the proposed hard surface. In addition, a new Graphic Panel depicting a historical image of the GSW will be incorporated into the design of the noise barrier will be visible from both sides and will add to the richness of the proposals.

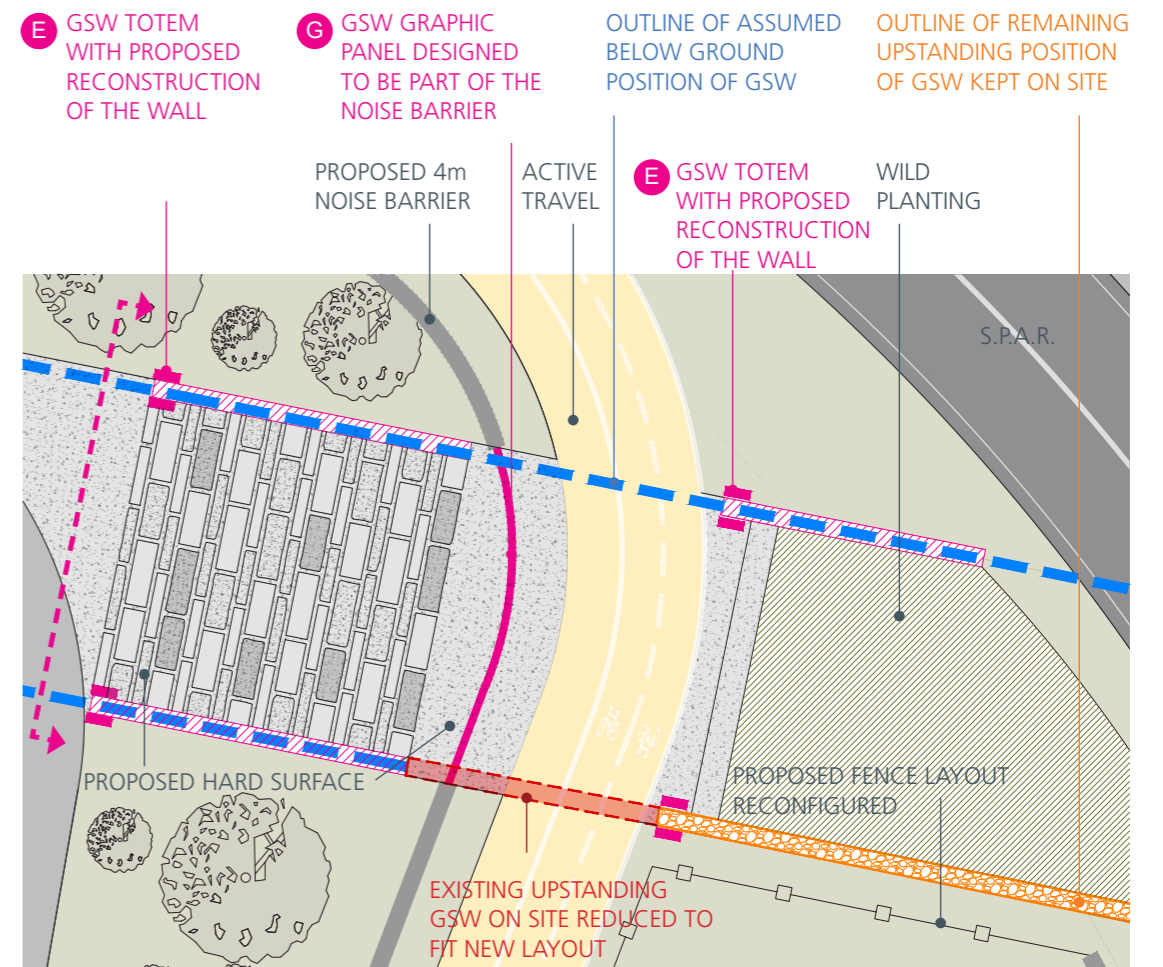


▲ Aerial view of new public space with proposed hard surface. Type E Totems with the reconstruction of the wall and an integrated Graphic Panel are proposed to represent the GSW, courtesy of BRIGHT



Indicative Proposals Only  
(Area not within Dublin Port ownership)

▲ Area 3 Key Plan



▲ Section 07-07  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW

- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Section 03



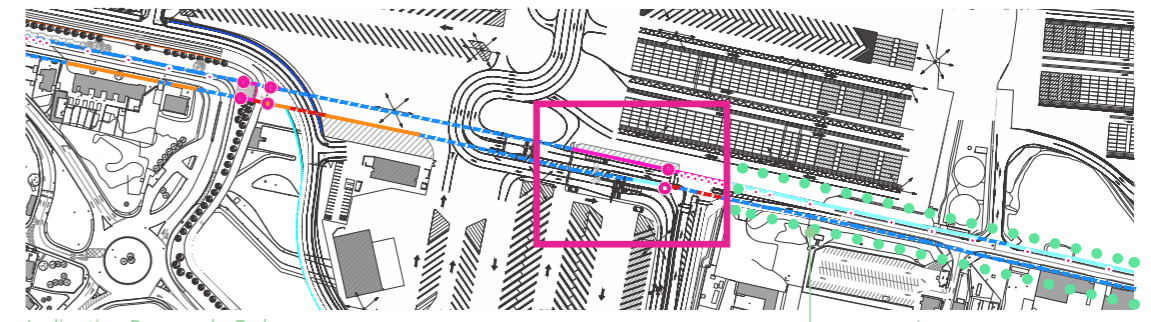
## Sections - Area 3 of Great South Wall: Proposed Area K

### Pigeon House Rd.

The western entrance to the proposed Area K Ro Ro Terminal is inaccessible to the public - whereas its eastern entrance (depicted in the image to the right) show public access which reconnects us to the Pigeon House Rd. This is the next publicly accessible site where the GSW currently sits below and above ground. Multiple laneways are proposed on this site, making the legibility of the GSW a challenge. Some minor remaining upstanding portions of the GSW are reduced to accommodate these new road layouts. To mitigate against this unavoidable loss, and to encourage and re-establish the GSW's presence, we propose the reuse of material from this demolition to reconstruct the stretch of currently missing GSW parapet, as well as repairing the existing upstanding portion of the GSW on site. These repairs and reconstruction will be done in accordance with strict conservation oversight and will be subtly different to the existing wall sections, to ensure legibility of old and new. Totems are situated on each side of the wall, serving as a visual link to the previous markers. Clusters of poles adjacent to the wall as well as the ground surface markers on the road show the direction in which the wall continues.



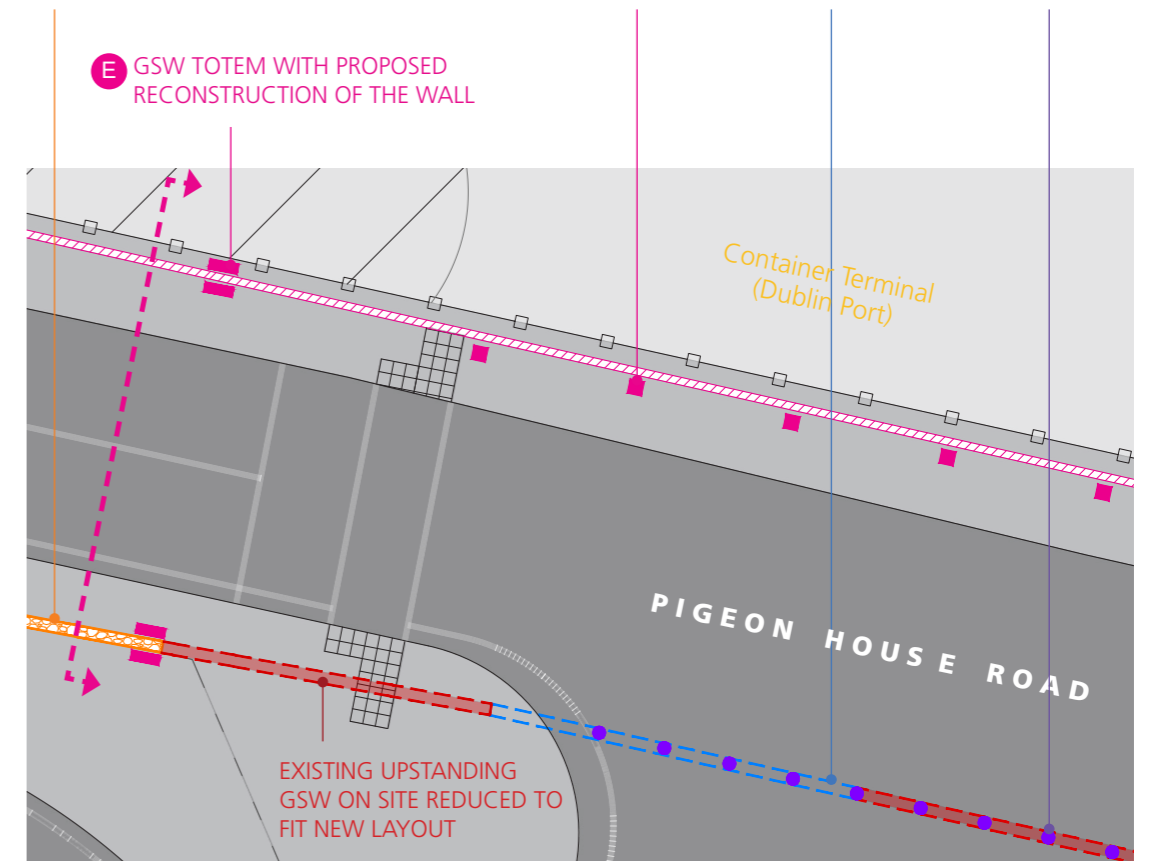
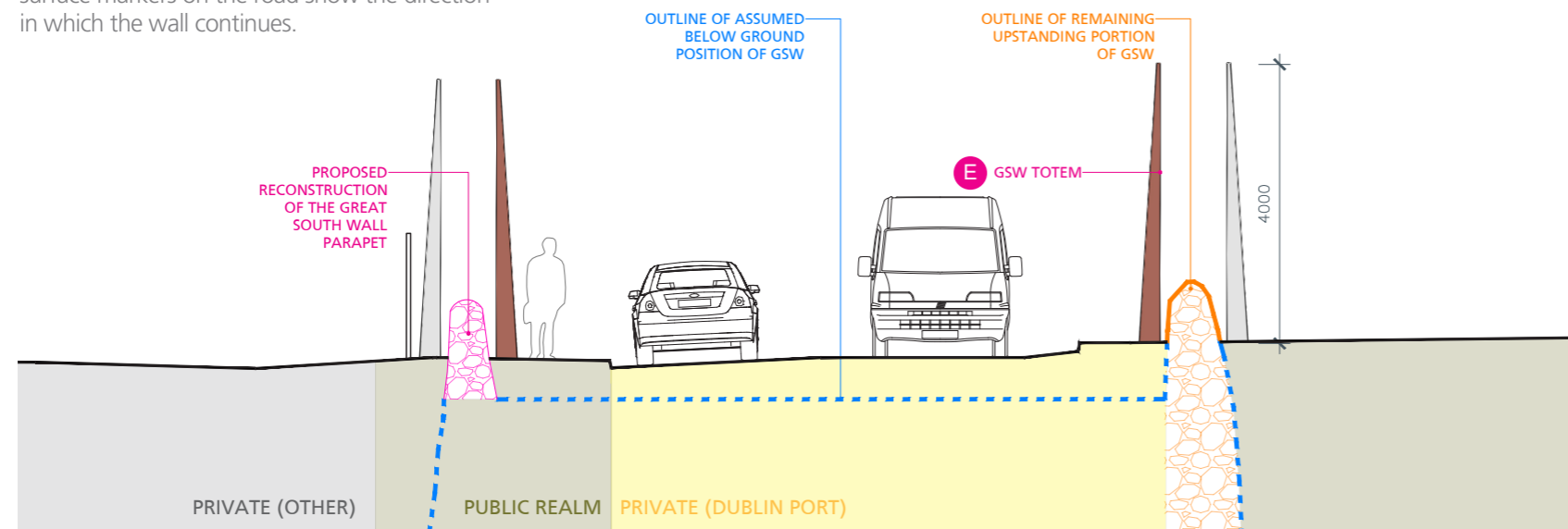
▲ Proposed GSW Totem & Pole Interpretation Elements Types E & D, courtesy of BRIGHT



Indicative Proposals Only  
(Area not within Dublin Port ownership)

▲ Area 3 Key Plan

■ OUTLINE OF REMAINING UPSTANDING POSITION OF GSW KEPT ON SITE  
● CLUSTER OF ILLUMINATING POLES MARKING THE GSW  
■ OUTLINE OF ASSUMED BELOW GROUND POSITION OF GSW  
● GSW GROUND SURFACE MARKINGS



▲ Section 09-09  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- ↑ Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers





Proposed Masterplan - Area 4 of Great South Wall: Proposed Area K to Pigeon House Precinct

To be read in conjunction with RPS Drawings (S26-HER-XX-DR-HE 100-1000 to 1006) for detailed Proposed Interventions of the Great South Wall

C

INTERMEDIATE MARKERS @ 30M CENTRES ALONG PIGEON HOUSE ROAD TO PIGEON HOUSE PRECINCT

PROPOSED SPAR ROAD RUNNING PARALLEL TO GREAT SOUTH WALL BOTH ABOVE AND BELOW GROUND. PROPOSED EARTHWORKS ASSOCIATED WITH ROAD WIDENING & NEW JUNCTION MAY IMPACT BELOW GROUND PORTIONS OF THE GREAT SOUTH WALL.

BROWN LINE INDICATES LATER CONSTRUCTION OF PIGEON HOUSE HARBOUR, FULLY INTACT TODAY, DESPITE AREA BEING LARGELY INFILLED TO HOUSE THE IRISH WATER DRAINAGE TANKS

Indicative Interpretation Proposals Only - not forming part of the 3FM Application



Keyplan showing extent of the Great South Wall

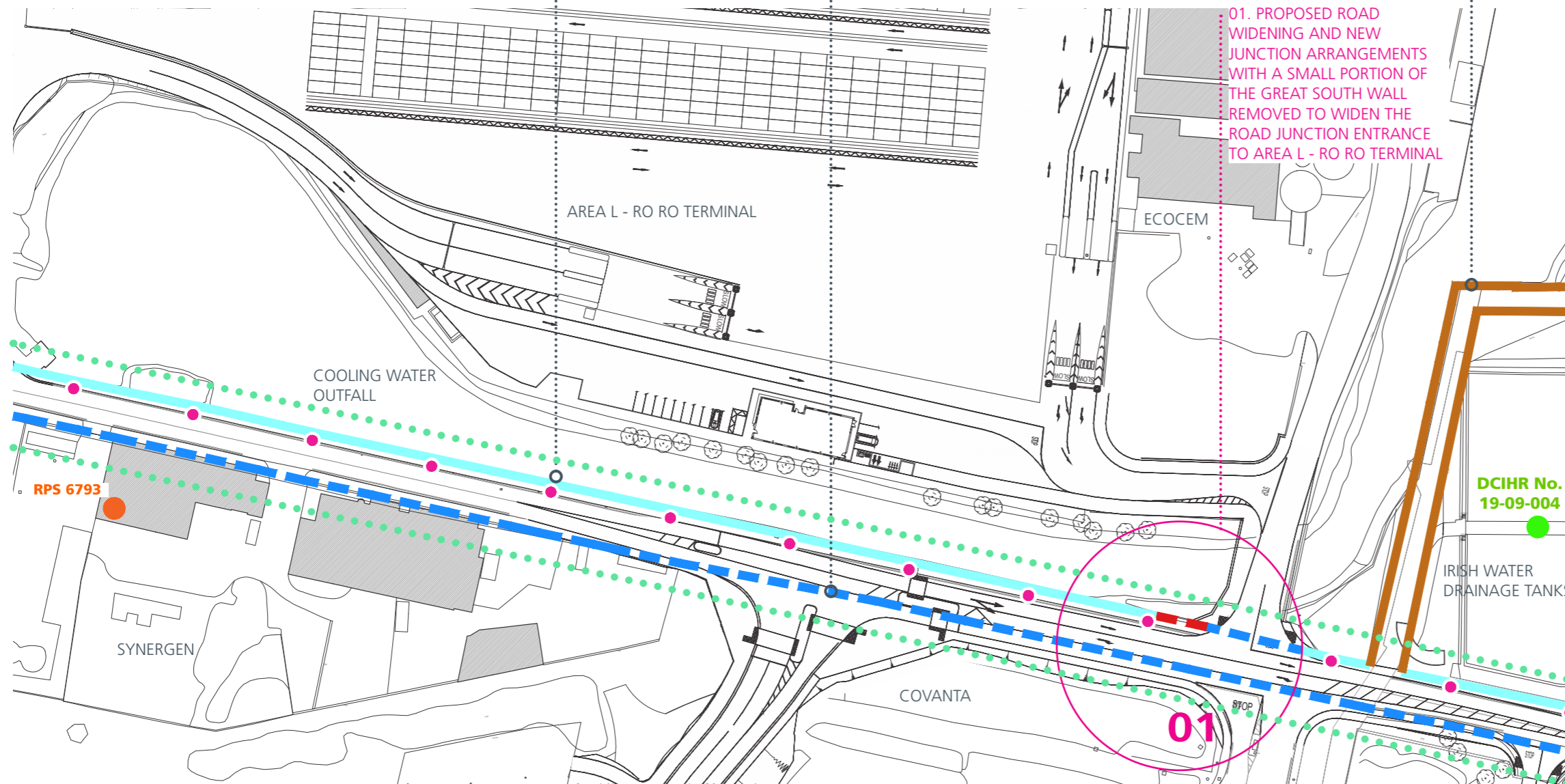
Pigeon House Rd. - Proposed Area K to Pigeon House Precinct

This stretch of road forms part of the current port industrial landscape, following the alignment of the Great South Wall. A significant portion of the northern parapet of the wall remains intact along the cooling water outfall, where its full height can be appreciated next to the water. These portions of GSW above ground will receive restoration treatment. In contrast, the majority of the southern parapet has been destroyed and is no longer visible above ground. Instead, various boundary treatments follow the former alignment, enclosing industrial sites to the south of Pigeon House Road.

Planned road widening and new junction arrangements may impact the below-ground sections of the southern wall, with only a small portion of GSW removed to widen the road junction entrance to Area L.

Given that this area is primarily accessed by port and industrial traffic, footfall is relatively low. Therefore, the proposal for this area is to continue a series of intermediate markers along the northern section of the wall at 30-metre intervals. This will maintain continuity and signal the wall's presence, ensuring its historical route remains legible.

These intermediate marker proposals are indicative only and not within Dublin Port ownership. It does not form part of the 3FM application and would require separate planning consent in the future.



01. PROPOSED ROAD WIDENING AND NEW JUNCTION ARRANGEMENTS WITH A SMALL PORTION OF THE GREAT SOUTH WALL REMOVED TO WIDEN THE ROAD JUNCTION ENTRANCE TO AREA L - RO RO TERMINAL

DCIHR No. 19-09-004

IRISH WATER DRAINAGE TANKS

01

Proposed Masterplan area 4 of the Great South Wall

Refer to **3FM EAIR Chapter 16 Appendices**

- National Inventory of Architectural Heritage (NIAH)
- Dublin City Industrial Heritage Record (DCIHR)
- Record of Protected Structures (RPS)
- ADCO

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- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface



Section 03



Sections - Area 4 of Great South Wall: Proposed Area K to Pigeon House Precinct

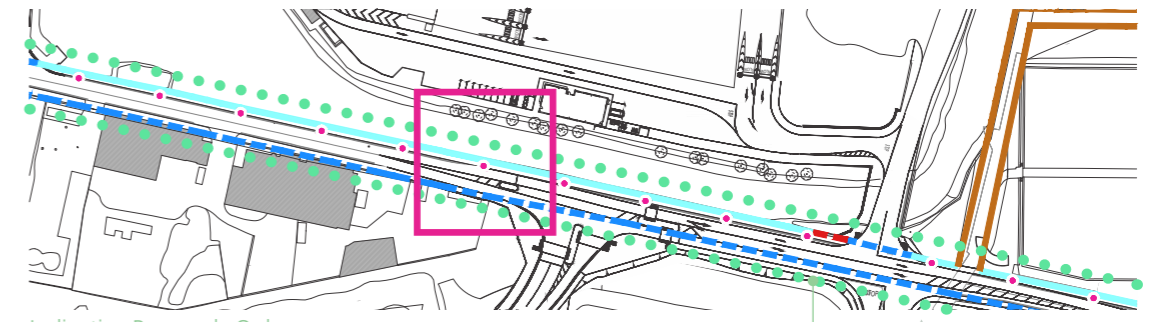
**Pigeon House Rd.**

To the right of this stretch of Pigeon House Road are various site entrances, including All Away Recycling & Skip Hire, PGSL Sound & Lighting, and Alan Doyle - Car Mechanic. The proposed road widening will involve removing and replacing the existing blockwork boundary walls. The narrow footpath and the access points to these warehouses make it challenging to locate GSW pole markers. Therefore, we propose continuing the GSW intermediate pole markers on the left-hand side of the road, where there is an existing GSW position at the water's edge. Additionally, this above-ground portion of the GSW will undergo restoration treatment as specified in the RPS Drawings (S26-HER-XX-DR-HE 100-1000 to 1006) and will be carried out under strict conservation oversight.



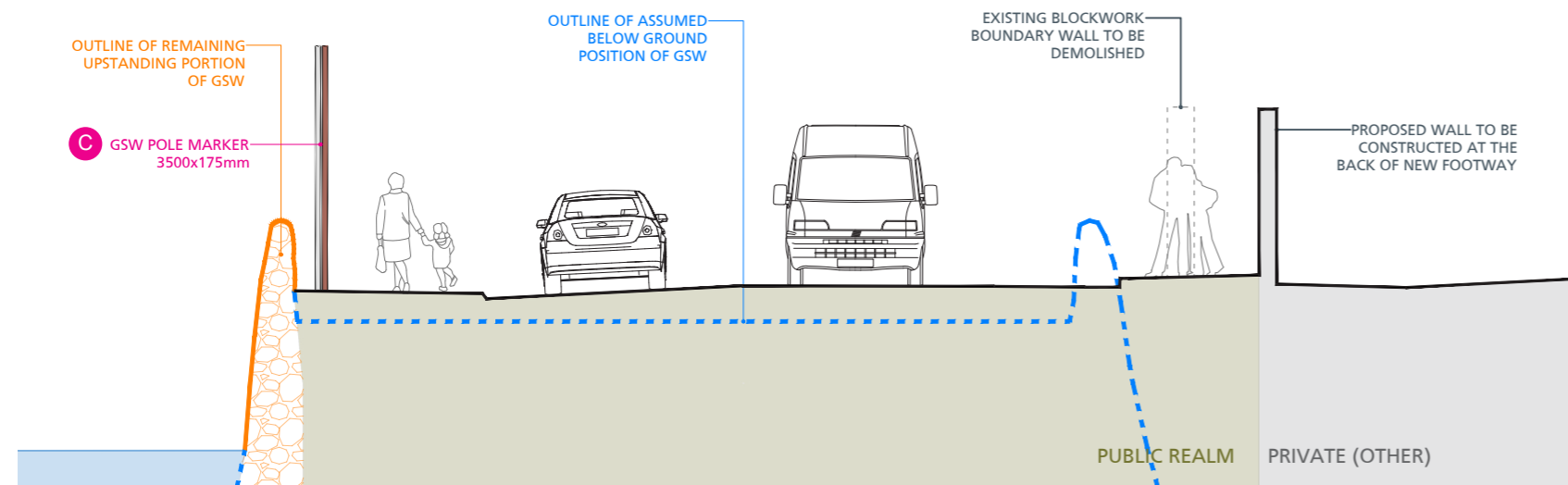
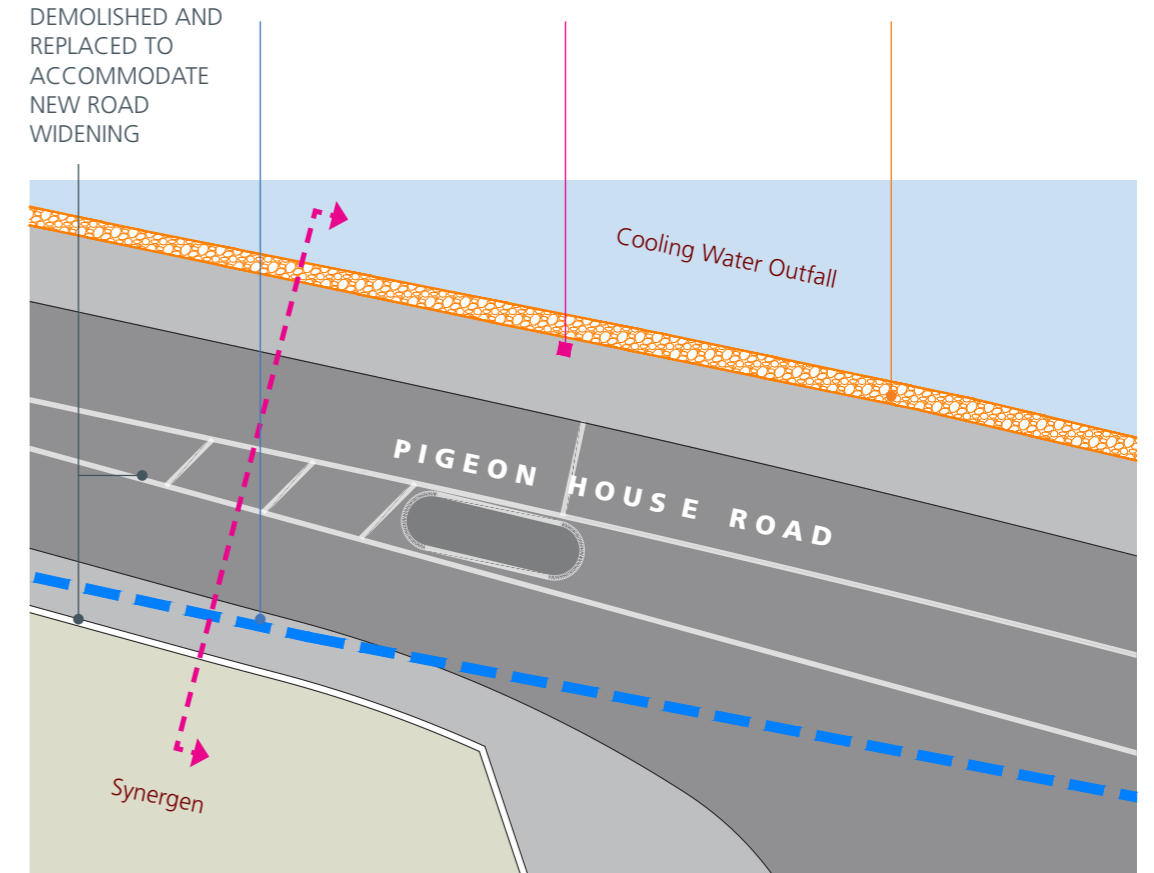
▲ Street View of Pigeon House Rd. with with proposed intermediate markers Type C at 30m intervals, courtesy of BRIGHT

As highlighted in the key plan, this area's proposals are indicative only and not within Dublin Port ownership. It does not form part of the 3FM application and would require separate planning consent in the future.



Indicative Proposals Only (Area not within Dublin Port ownership) ▲ Area 4 Key Plan

EXISTING BLOCKWORK BOUNDARY WALL DEMOLISHED AND REPLACED TO ACCOMMODATE NEW ROAD WIDENING  
 OUTLINE OF ASSUMED BELOW GROUND POSITION OF GSW  
 POLES MARKING THE GSW WITH GREATER DISTANCE IN BETWEEN  
 OUTLINE OF REMAINING UPSTANDING PORTION OF GSW



▲ Section 11-11 1:100

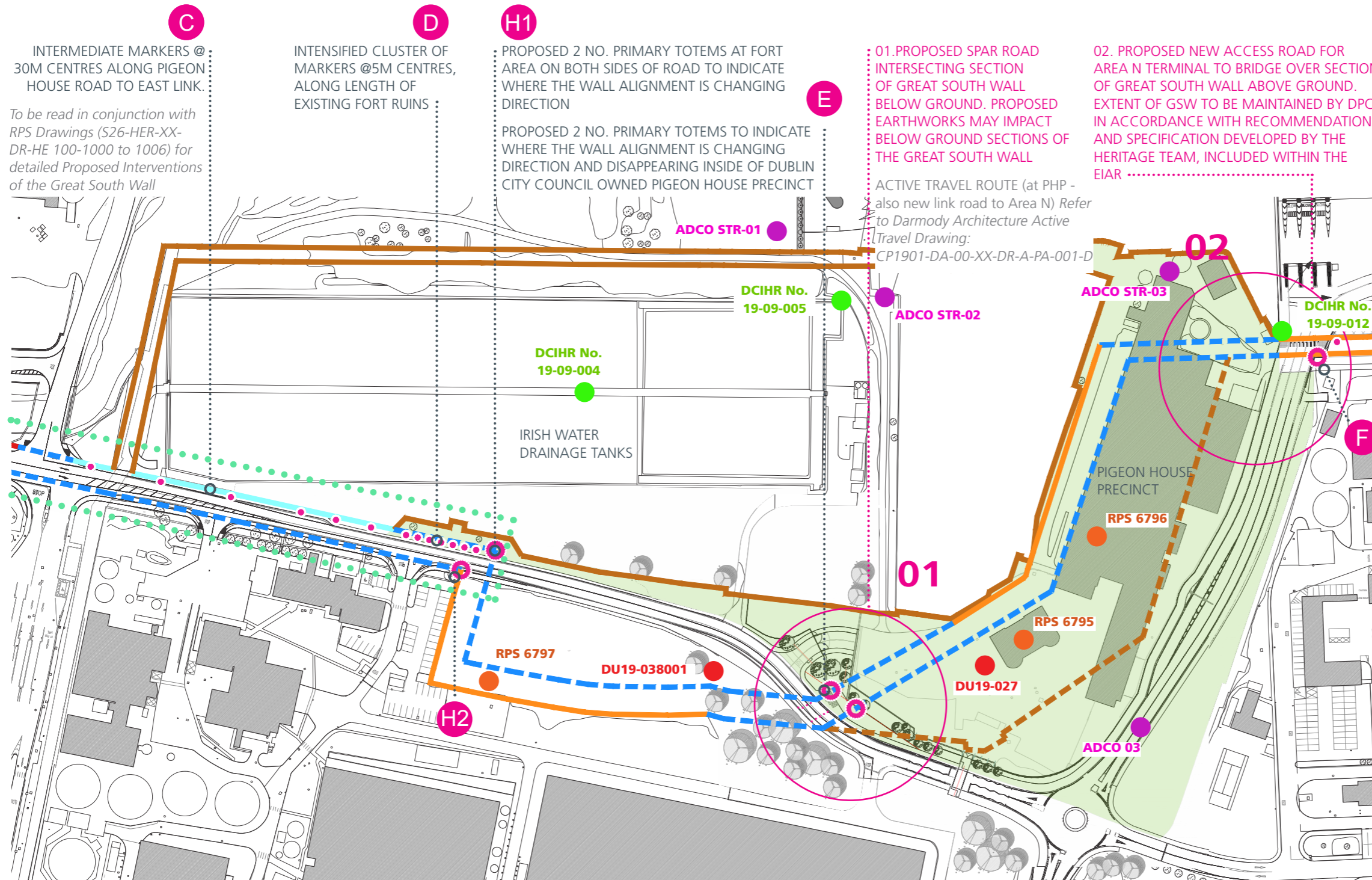
- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Section 03



Proposed Masterplan - Area 5 of Great South Wall: Pigeon House Precinct



Indicative Interpretation Proposals Only - not forming part of the 3FM Application



Keyplan showing extent of the Great South Wall

**Pigeon House Precinct**

This comprises the area bounded by the Harbour, the GSW/Pigeon House Road; and the enclosing wall of the Fort. It is not absolutely certain what form predated the Harbour and reclamation area for the Fort, but it did contain a place to berth ships.

Public access diminishes significantly as one approaches the Pigeon House Precinct. The initial stretch of road leading to the fort reveals the northern parapet of the Great South Wall, while the southern parapet is obscured and replaced by a new wall separating private properties from Pigeon House Road.

While the Great South Wall maintained a linear path in previous sections, it begins to change direction in this area, from the fort ruins to the new travel route and proposed bridge access at the Pigeon House Precinct. Much of the Great South Wall re-emerges above ground in inaccessible private areas, often visible along the water's edge but hidden from the footway.

Primarily accessed by port and industrial traffic, this area sees relatively low pedestrian footfall, similar to Area K. As part of our proposal, pole markers will continue along Pigeon House Road, becoming more densely clustered near the fort area. Additionally, special totem markers will be introduced to indicate changes in the wall's direction, enhancing navigation and appreciation of this historical feature.

To be read in conjunction with ACTIVE TRAVEL DRAWINGS (at PHP - also new link road to Area N) Refer to Darmody Architecture Active Travel Drawing: CP1901-DA-00-XX-DR-A-PA-001-D

- Refer to **3FM EAIR Chapter 16 Appendices**
- National Inventory of Architectural Heritage (NIAH)
  - Dublin City Industrial Heritage Record (DCIHR)
  - Record of Protected Structures (RPS)
  - ADCO

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- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface



Section 03

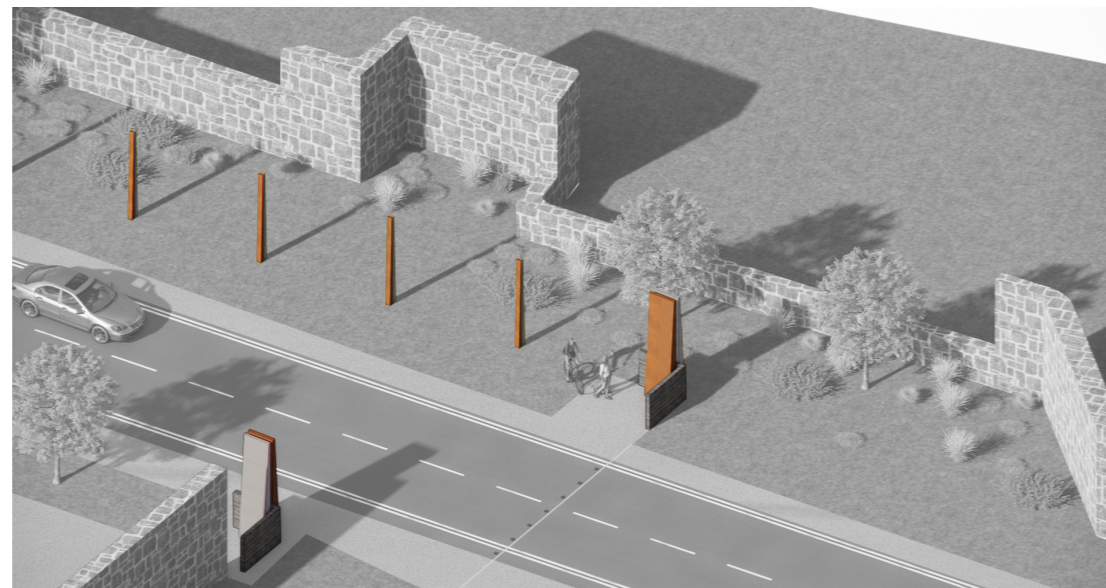


## Sections - Area 5 of Great South Wall: Pigeon House Precinct

### Pigeon House Rd. - Fort

The GSW continues directly down Pigeon House Road up to the point where the wall sharply changes direction. This area, adjacent to the fort and across the road, offers a generous space ideal for a cluster of illuminating poles, which will give the space a distinctive character at night. Special L-shaped totems will outline the corners where the wall changes direction, adding a unique visual element.

These totems will be installed on new hard surfaces, with ground surface markings used to indicate the continuation of the wall across the road. The landscape design will need to be carefully integrated into these proposals to ensure aesthetic harmony and historical sensitivity. Given the site's significance, this project will benefit from a collaborative effort involving Dublin City Council and other stakeholders, ensuring that all enhancements respect and highlight the historical and cultural importance of the area.



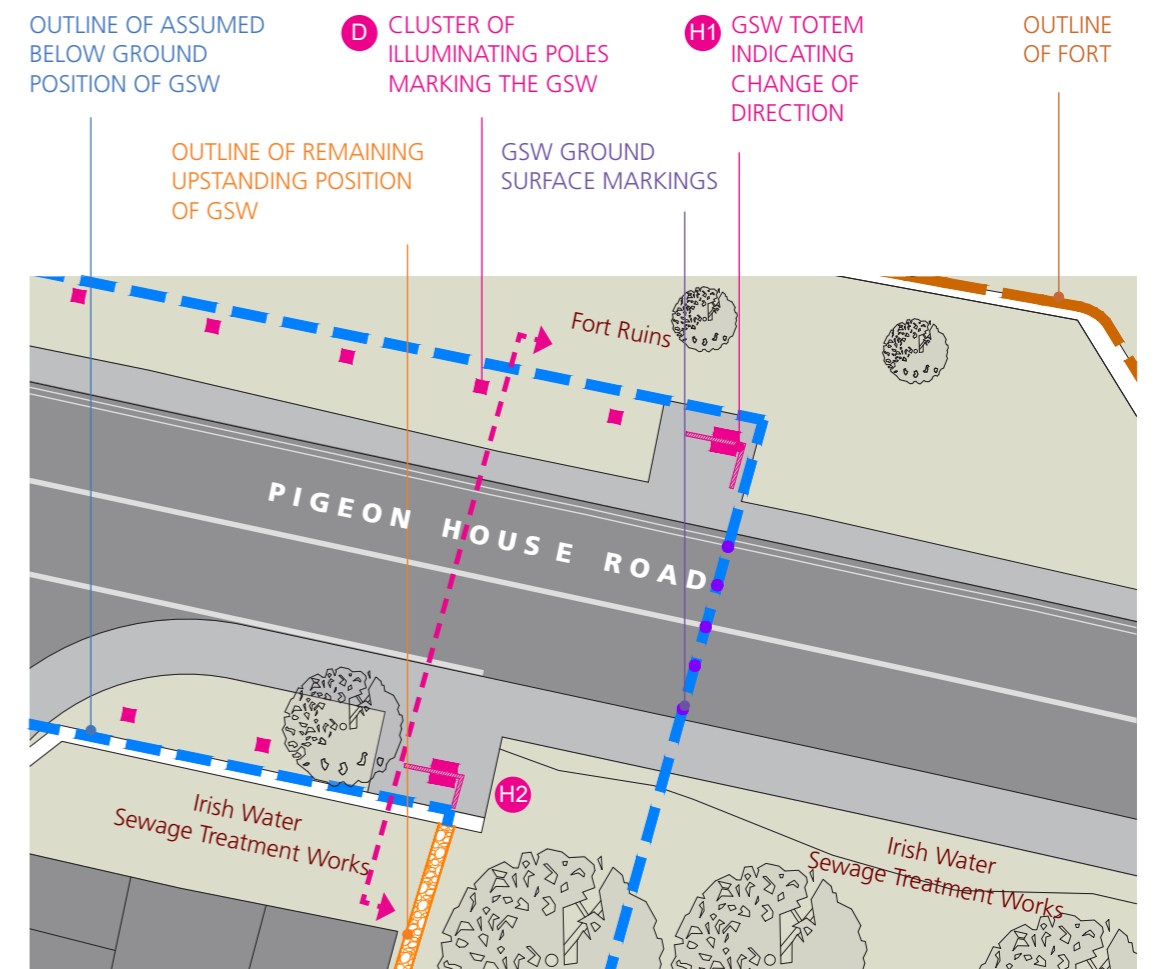
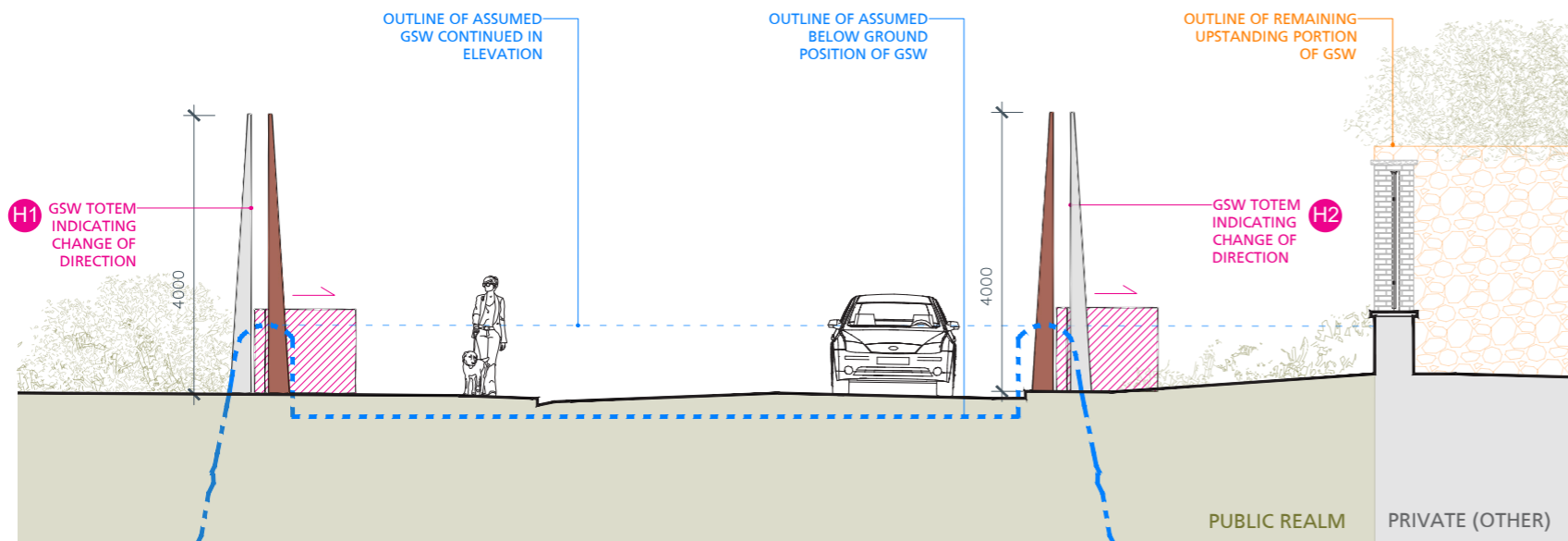
As highlighted in the key plan, this area's proposals are indicative only and not within Dublin Port ownership. It does not form part of the 3FM application and would require separate planning consent in the future.

▲ Proposed GSW ground surface markers, Totem & Pole Interpretation Elements Types D, H1 & H2, courtesy of BRIGHT



Indicative Proposals Only  
(Area not within Dublin Port ownership)

▲ Area 5 Key Plan



▲ Section 14-14  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW

- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers

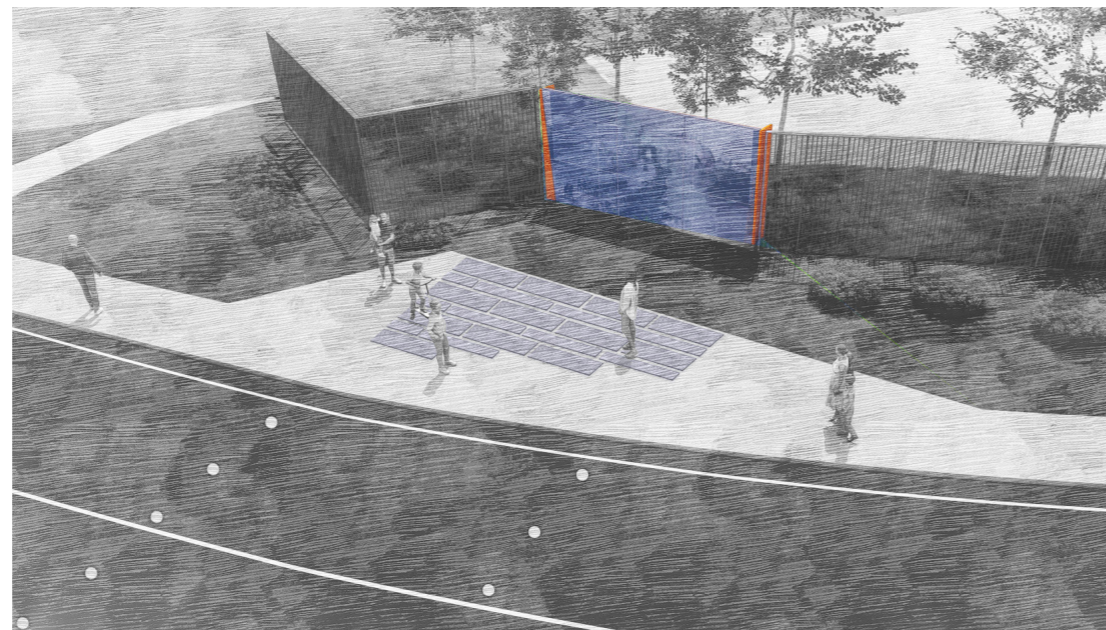




## Sections - Area 5 of Great South Wall: Pigeon House Precinct

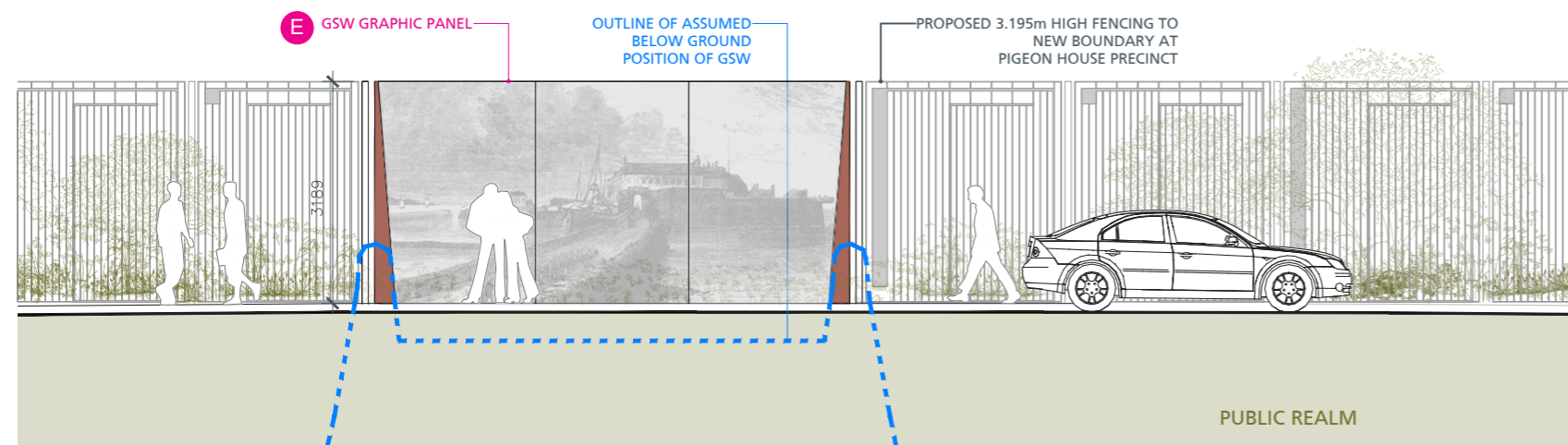
### Pigeon House Rd. - Active Travel

This section is the last site in which the GSW's assumed outline below ground reappears (after its sharp change in direction at the fort). The wall crosses the road perpendicularly and continues beyond the private fenced Pigeon House Precinct (PHP), in the ownership of Dublin City Council. It is currently inaccessible to the public, but this could change with potential future proposals. For this reason, we would encourage highlighting the presence of the GSW which could potentially continue within the PHP as part of these proposals. Like the Noise Barrier in Area 04, we are proposing a GSW Graphic Panel integrated within the proposed fencing - taking into consideration the constraints of sight lines. The fence layout is reconfigured for the Graphic Panel to be positioned in alignment of the GSW. This would paint the correct direction in which the wall continues through the PHP grounds. Salvaged stones would be reused for the new hard surface representing the GSW, while ground surface markers outline the assumed below ground position of the GSW as it crosses the road.



▲ GSW Graphic Panel integrated into the proposed fence - reconfigured to suit the alignment of the assumed below ground position of GSW

To be read in conjunction with ACTIVE TRAVEL DRAWINGS (at PHP - also new link road to Area N) Refer to Darmody Architecture Active Travel Drawing: CP1901-DA-00-XX-DR-A-PA-001-D



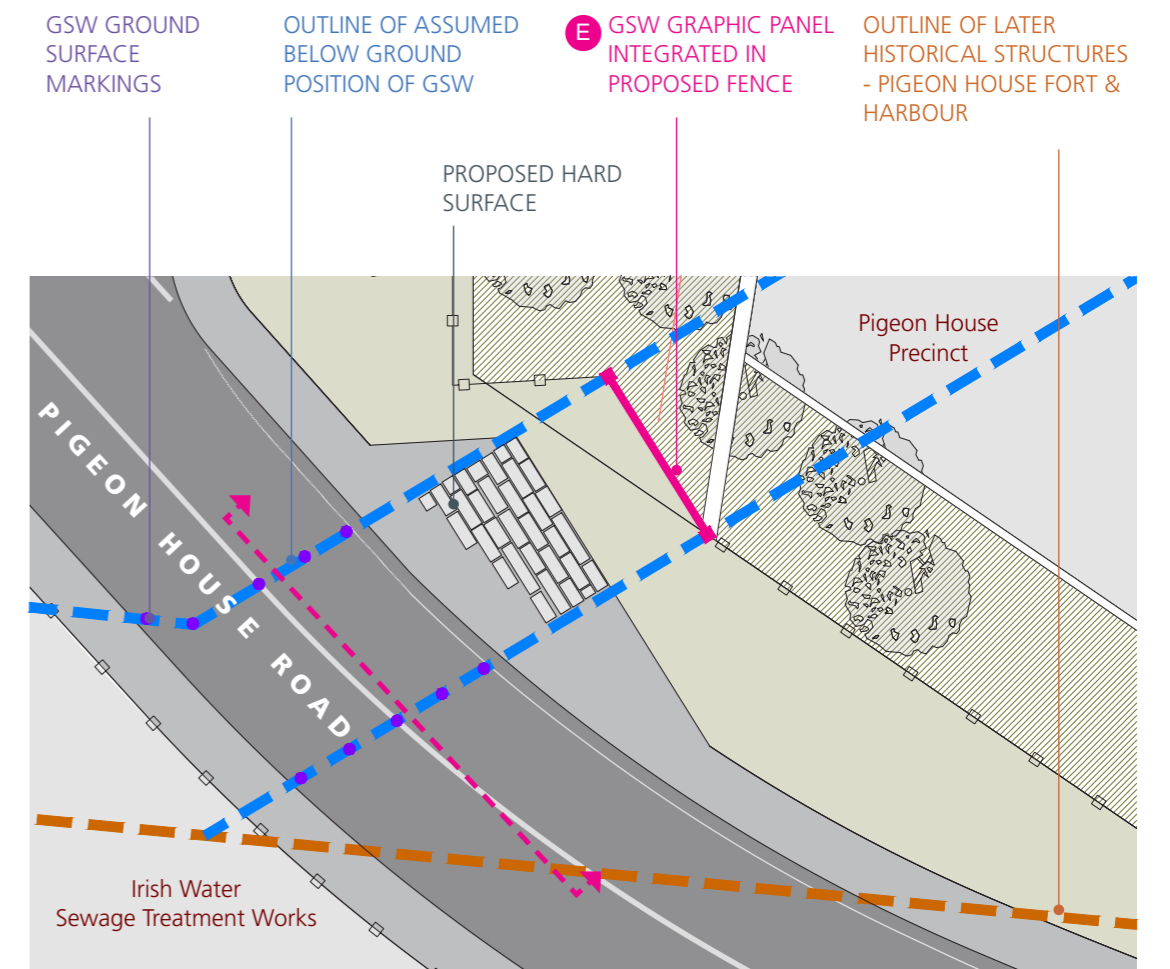
▲ Section 15-15  
1:100

- GSW Visible Above Ground
- - - GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



Indicative Proposals Only  
(Area not within Dublin Port ownership)

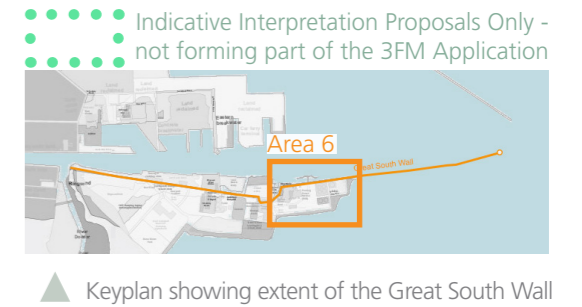
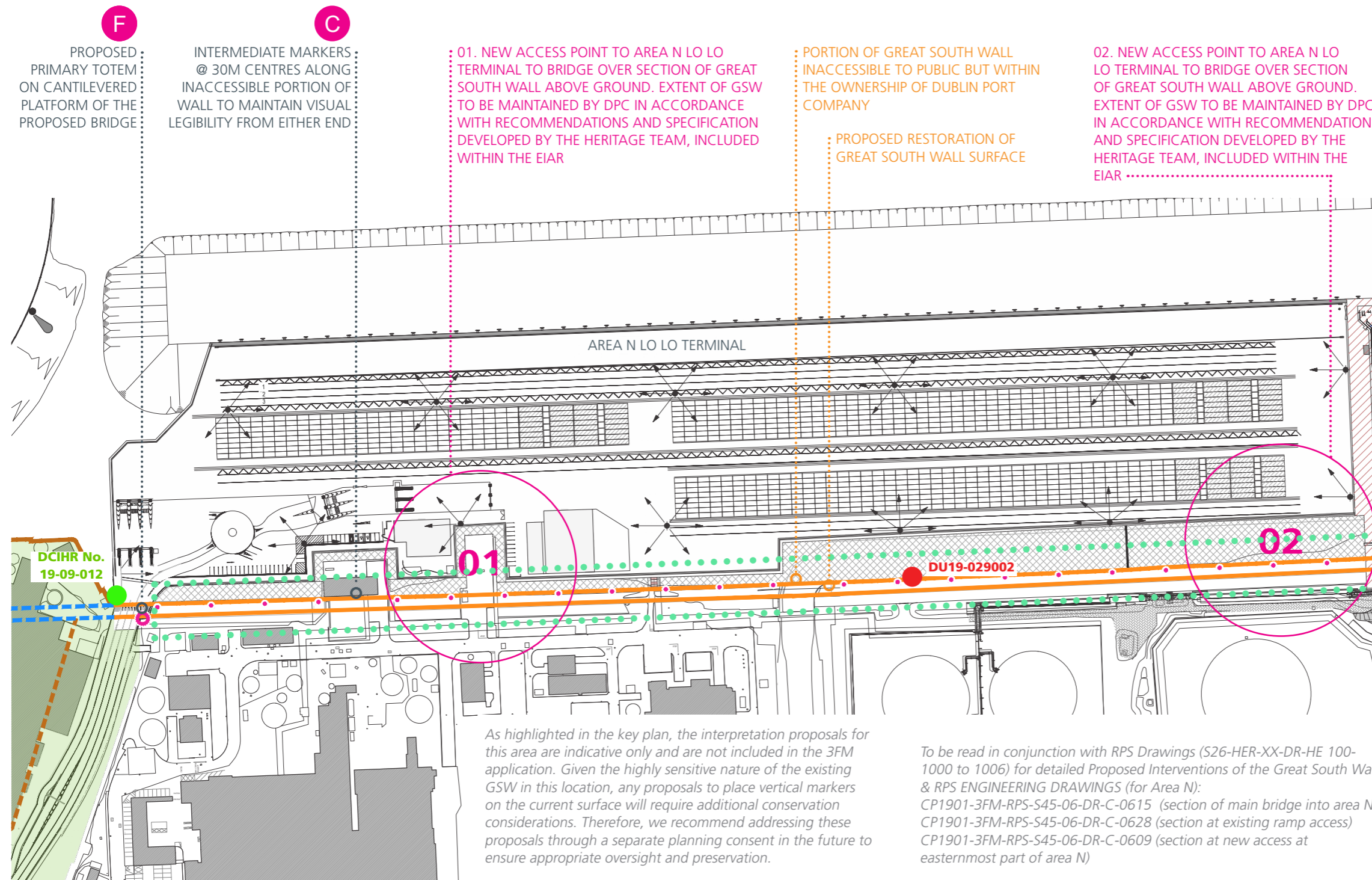
▲ Area 5 Key Plan



### Section 03



Proposed Masterplan - Area 6 of Great South Wall: Proposed Area N



Proposed Area N - Lo Lo Terminal

The area under consideration includes a portion of the Great South Wall which is currently inaccessible to the public but owned by Dublin Port Company. The proposed development introduces a new access road leading to a bridge that will provide entry to the new Area N Lo-Lo Terminal - which is to be constructed on piles to the north of the existing land boundary. Given the current lack of public access, we recommend installing pole markings along the edge of the Great South Wall to emphasize its presence and continue visible connectivity.

The development plan outlines the creation of a new access point to the Area N Lo-Lo Terminal, which will include a bridge over an above-ground section of the Great South Wall. Dublin Port Company will maintain this section of the wall in accordance with recommendations and specifications developed by the Heritage Team, as detailed in the Environmental Impact Assessment Report (EIAR). These new proposed bridge access points, in accordance to RPS drawings, are well considered and avoid interaction with the existing Great South Wall. These bridge structures are built to sail over the wall or utilize existing crossing points. This long stretch of the Great South Wall will also receive restoration treatment on its ground surface in accordance with strict conservation oversight.

Refer to **3FM EIAR Chapter 16 Appendices**

- National Inventory of Architectural Heritage (NIAH)
- Dublin City Industrial Heritage Record (DCIHR)
- Record of Protected Structures (RPS)
- ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface

▲ Proposed Masterplan area 6 of the Great South Wall

Section 03

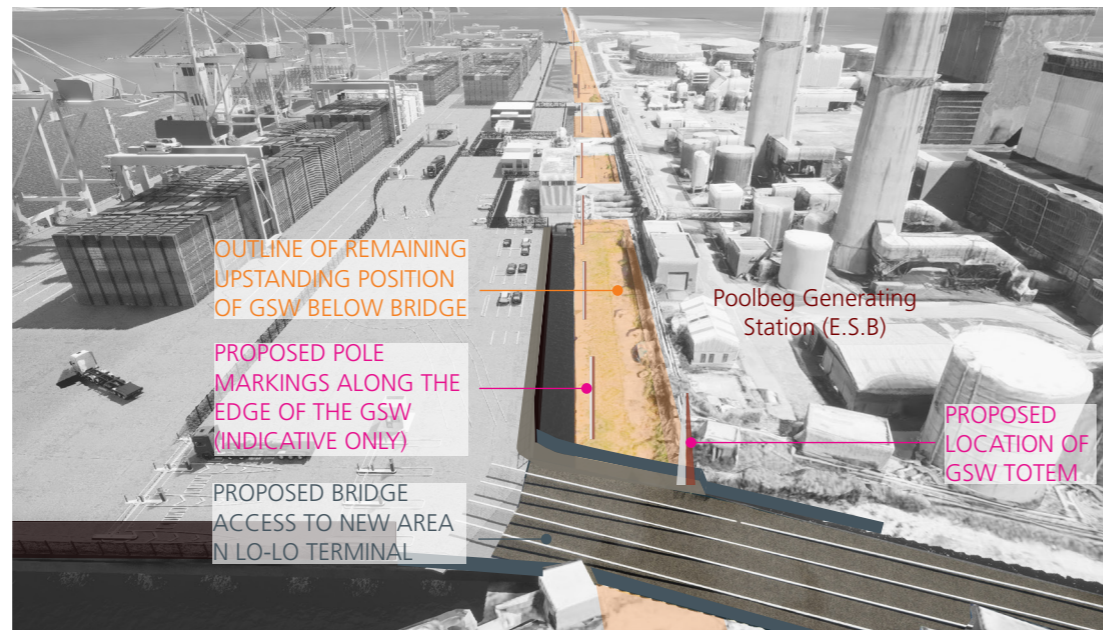


## Sections - Area 6 of Great South Wall: Proposed Area N

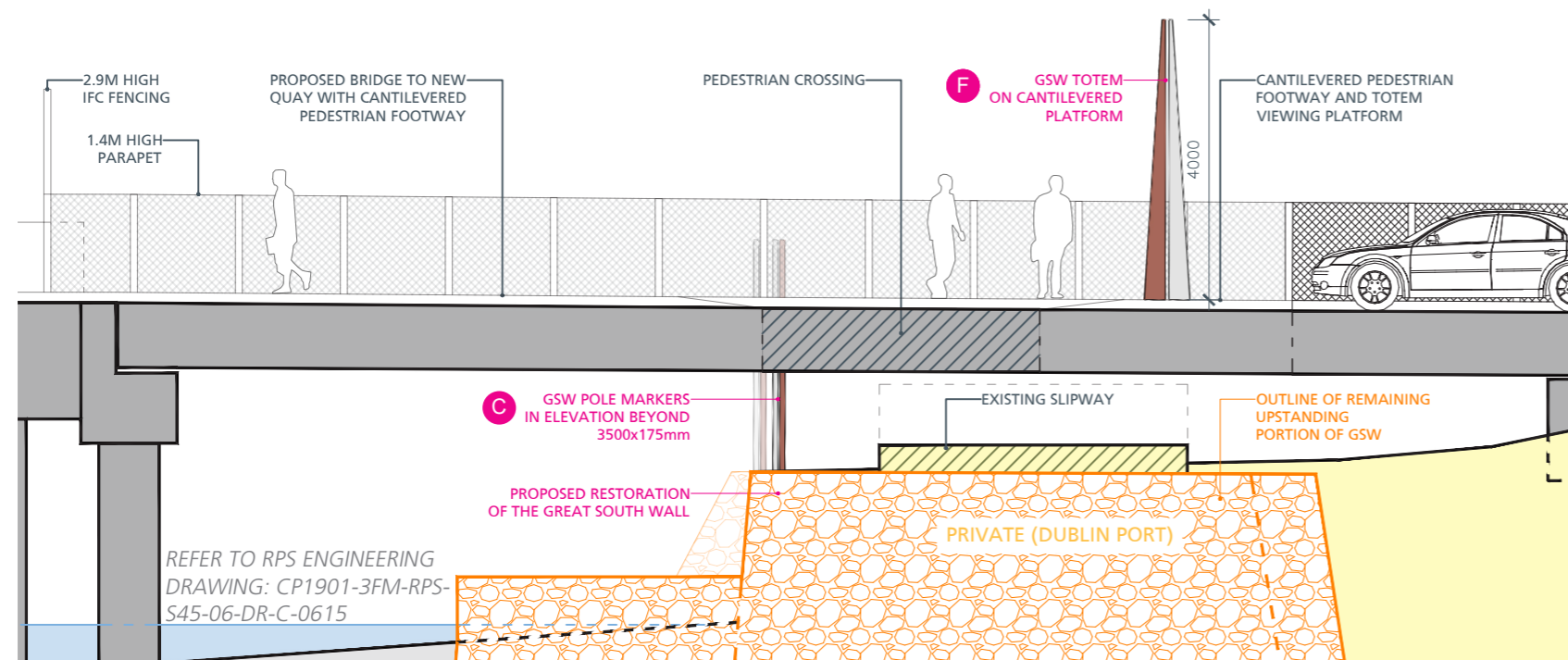
### Main Bridge Access to New Area N Lo-Lo Terminal

The new road leads to a bridge providing access to the Area N Lo-Lo Terminal. This proposed access has been carefully planned to avoid impacting the Great South Wall. The bridge will span over the wall without touching the existing structure, preserving its integrity.

While there is currently no public access to this point, the increased foot and car traffic for staff entering the terminal calls for thoughtful enhancements. We propose installing an interpretative totem on the new bridge, adjacent to the pedestrian crossing aligned with the wall below. This totem, enjoying an elevated position, will mark the wall's location at the last accessible point before reaching the privately controlled Area N Lo-Lo Terminal. This addition will not only enhance navigation but also highlight the historical significance of the Great South Wall.

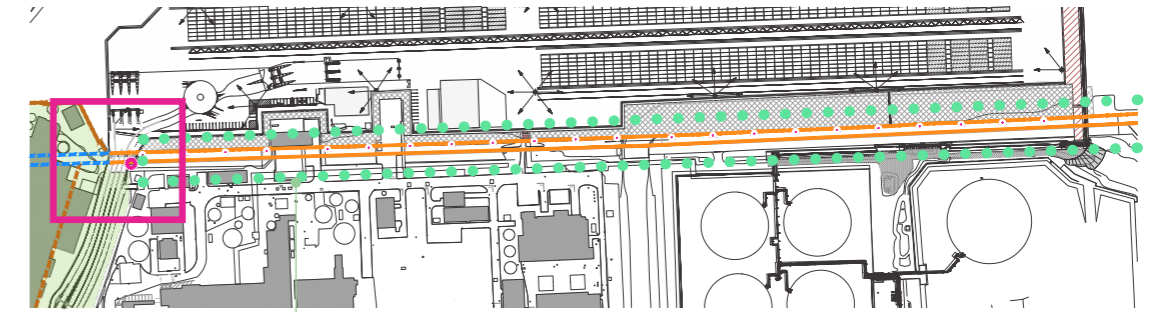


▲ RPS 3FM Aerial Visualization



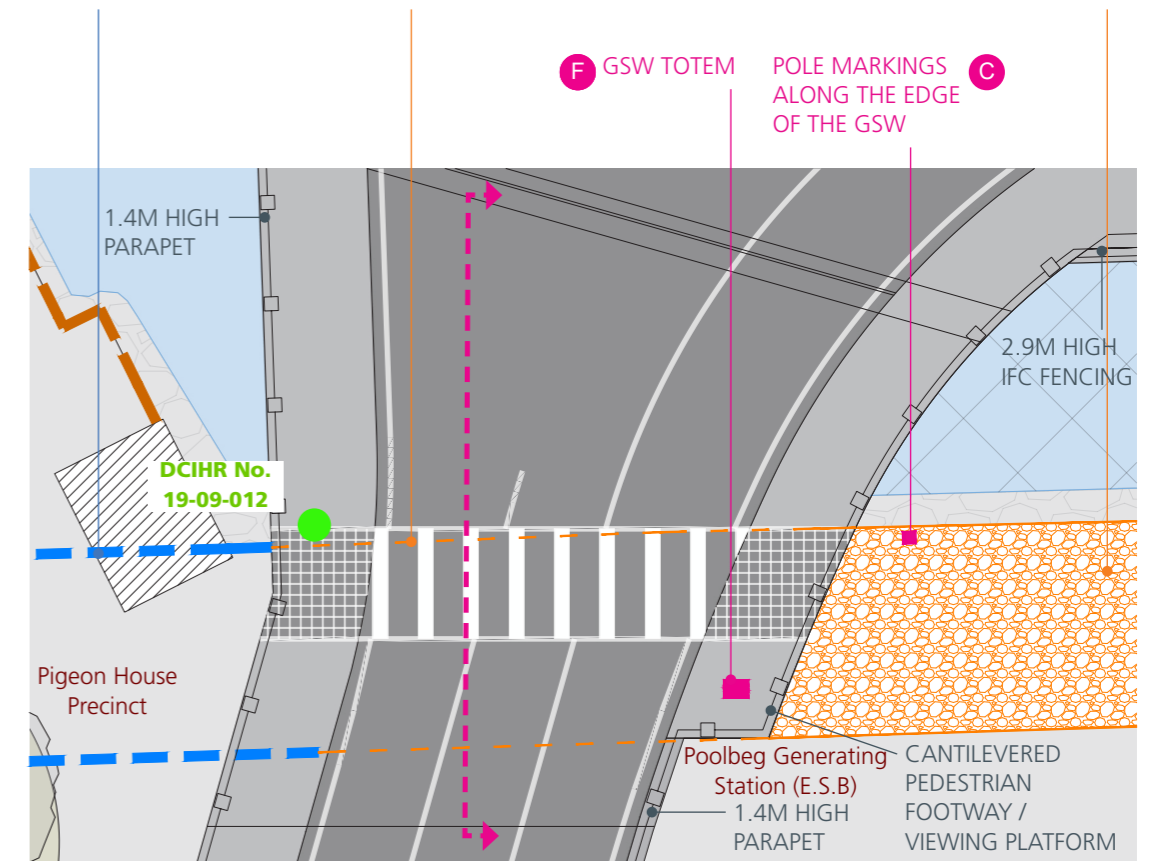
▲ Section 16-16  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



▲ Area 5 Key Plan

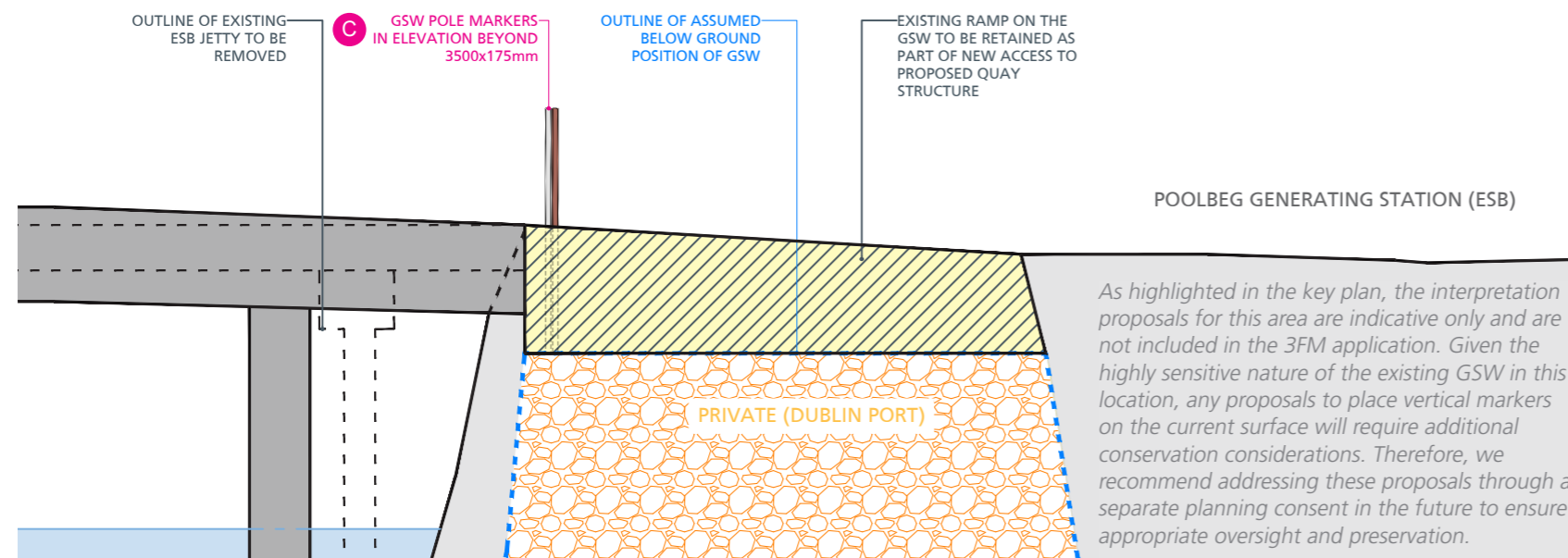
- OUTLINE OF ASSUMED BELOW GROUND POSITION OF GSW
- OUTLINE OF REMAINING UPSTANDING POSITION OF GSW BELOW BRIDGE
- OUTLINE OF REMAINING UPSTANDING POSITION OF GSW BELOW BRIDGE LEVEL



### Section 03

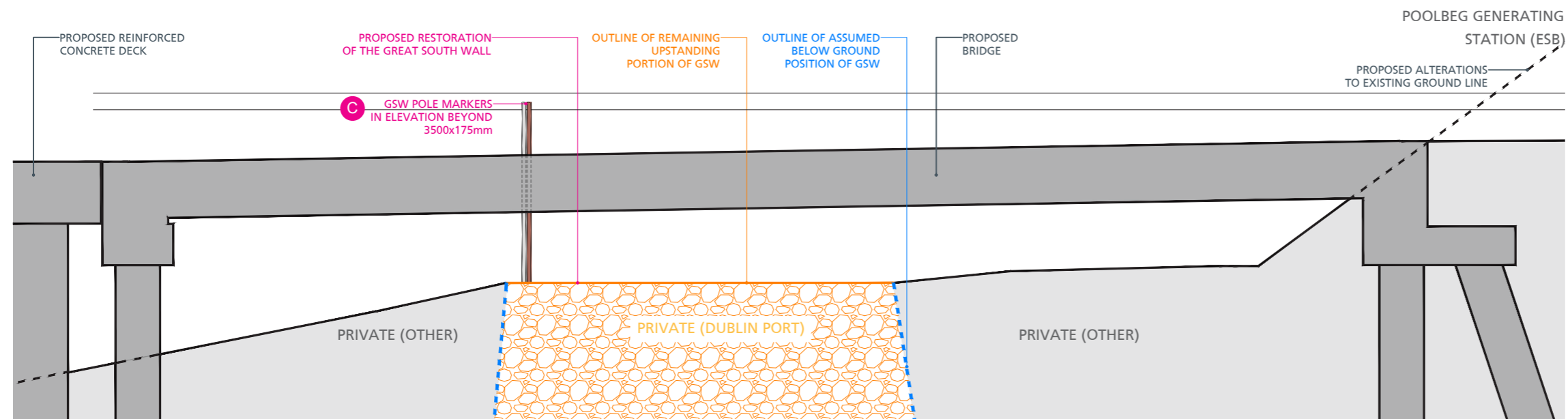


Sections - Area 6 of Great South Wall: Proposed Area N



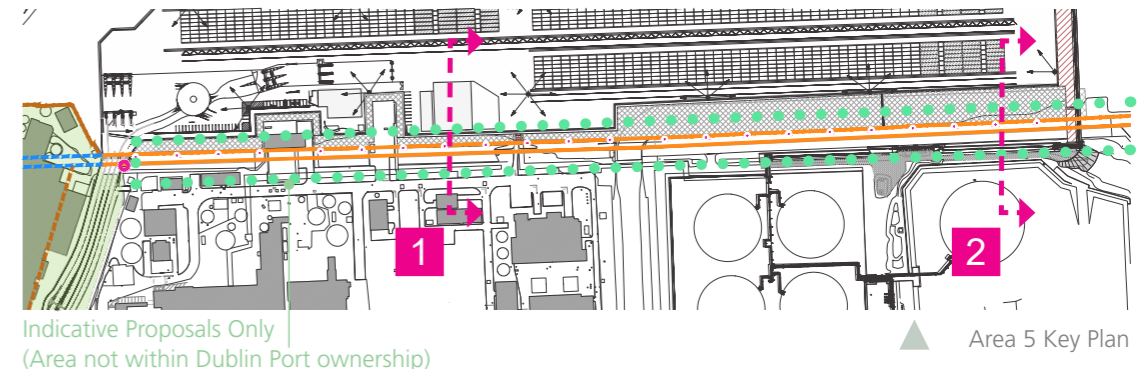
SECTION 1 Proposed Blue Light Ramp Access  
1:100

REFER TO RPS ENGINEERING DRAWING:  
CP1901-3FM-RPS-S45-06-DR-C-0628



SECTION 2 Proposed Access at ESB Jetty  
1:100

REFER TO RPS ENGINEERING DRAWING:  
CP1901-3FM-RPS-S45-06-DR-C-0609



This section of the Great South Wall, owned by Dublin Port Company, currently provides private access to the Poolbeg Generating Station (ESB) and the proposed new Area N LO-LO Terminal, remaining inaccessible to the public. The wall is intact along its full length as a ground surface, with no upstanding parapet walls, similar to the portion extending to Poolbeg Lighthouse. The last public access point is at the main bridge pedestrian crossing to the west, and it is fenced off from the publicly accessible portion of the GSW at the eastern end.

It is proposed for the entire ground surface of this length of the GSW to undergo restoration in accordance with strict conservation oversight. Additionally, there are two proposed crossing points over to the new Area N LO-LO Terminal which have been designed to minimize impact upon the GSW as follows:

**Proposed Blue Light Ramp Access**

This proposed access to the new Area N LO-LO Terminal is strategically located at an existing ramp access. The existing ramp on the Great South Wall will be retained as part of this new bridge access, ensuring no impact on the remaining upstanding portion of the Great South Wall.

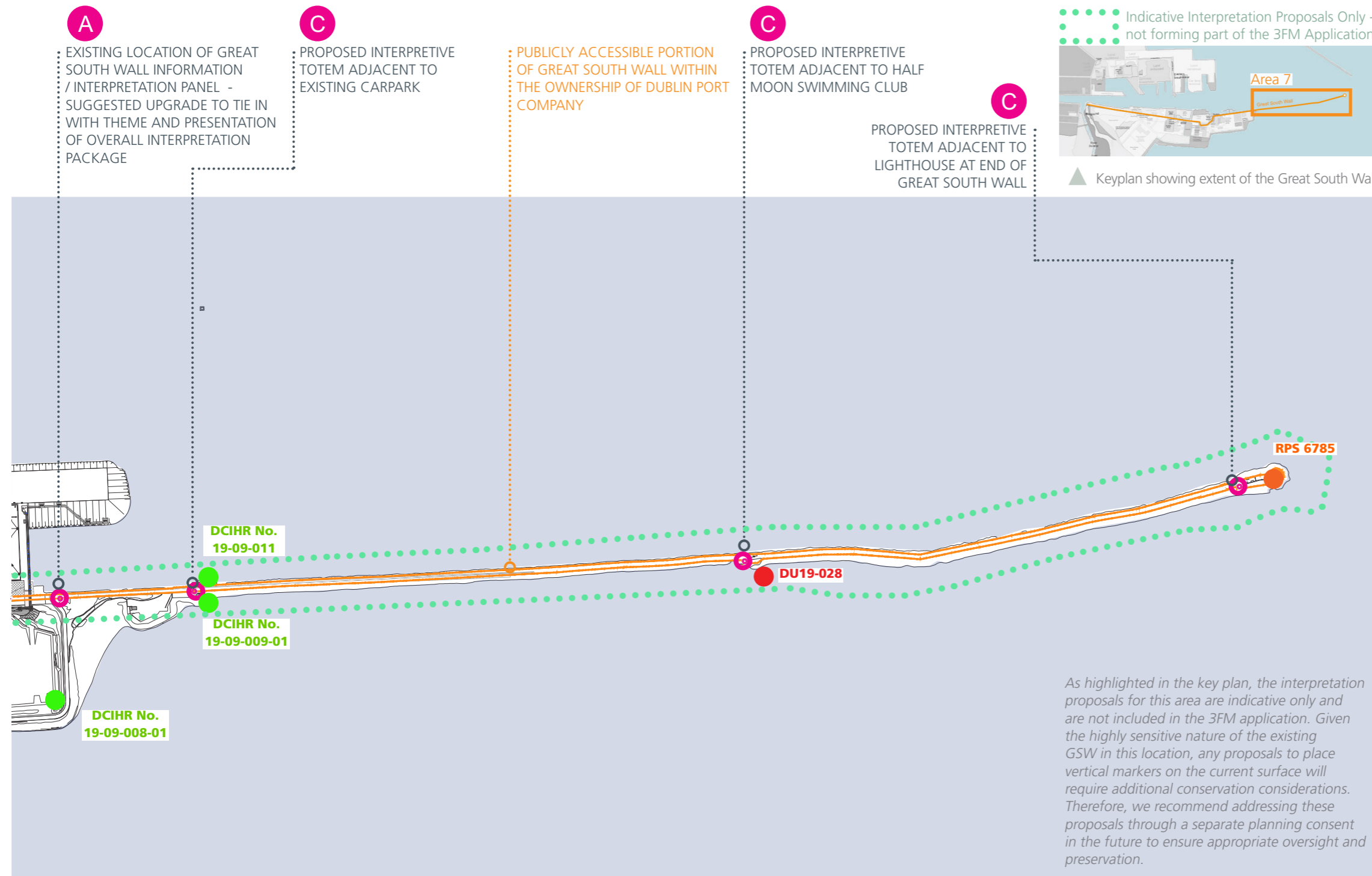
**Proposed Access at ESB Jetty**

Similar to the main bridge access, this section of the new access at the easternmost part of Area N will bridge over the Great South Wall. Designed with careful consideration, this proposed bridge structure avoids interaction with the wall. Consequently, the visibility of the historic fabric is preserved and further enhanced with the proposed restoration of its surface.

These proposals have been carefully designed to respect and preserve the integrity of the Great South Wall while facilitating necessary access to the new terminal, ensuring that the historical significance of the wall is maintained and highlighted.



Proposed Masterplan - Area 7 of Great South Wall: Existing South Wall to Lighthouse



**Pigeon House Walk**

The easternmost part of the Great South Wall begins with pedestrian and vehicle access leading to a car park, which experiences significant foot traffic from visitors who prefer walking through the sand dunes—a more popular route to the lighthouse. To enhance the visitor experience, we propose installing three interpretive markers at key nodal points along the wall. We recognize that placing markers along this cherished section of the GSW may be seen as controversial. However, these markers are essential for fostering public understanding of the wall's historical significance. By introducing the markers here, visitors will be able to recognize and connect similar markers in other areas, even where the upstanding portions of the wall are no longer present. This continuity will help bridge the historical narrative of the Great South Wall across different sections, enriching the overall visitor experience.

To begin, we propose replacing the existing information panel at the main access point with an integrated GSW totem. This will seamlessly blend historical context with modern interpretation.

Adjacent to the car park, where vehicular access ends and foot traffic is high, we suggest erecting a marker. Another marker is proposed near the Half Moon Swimming Club, giving visitors a sense of progression along their walk. Finally, the last marker, located by the lighthouse, will signify the culmination of the journey, allowing visitors to appreciate the distance they have covered on the Great South Wall.

These enhancements will be consistent with the overall theme and presentation of the interpretation package, significantly improving visitor engagement and understanding. By creating a cohesive and informative experience, we aim to deepen the public's connection to the historical and cultural significance of the Great South Wall.

*As highlighted in the key plan, the interpretation proposals for this area are indicative only and are not included in the 3FM application. Given the highly sensitive nature of the existing GSW in this location, any proposals to place vertical markers on the current surface will require additional conservation considerations. Therefore, we recommend addressing these proposals through a separate planning consent in the future to ensure appropriate oversight and preservation.*

- Refer to **3FM EAIR Chapter 16 Appendices**
- National Inventory of Architectural Heritage (NIAH)
  - Dublin City Industrial Heritage Record (DCIHR)
  - Record of Protected Structures (RPS)
  - ADCO

- GSW - Original wall intact above ground and visible
- GSW - Existing above ground structure no longer present (assumed position below ground indicated)
- GSW - Proposed removal of above ground structure (parapet walls)
- GSW - Proposed reconstruction of above ground parapet walls
- GSW - Proposed restoration / increase in height of existing GSW parapet / proposed restoration of existing ground surface

▲ Proposed Masterplan area 7 of the Great South Wall

Section 03

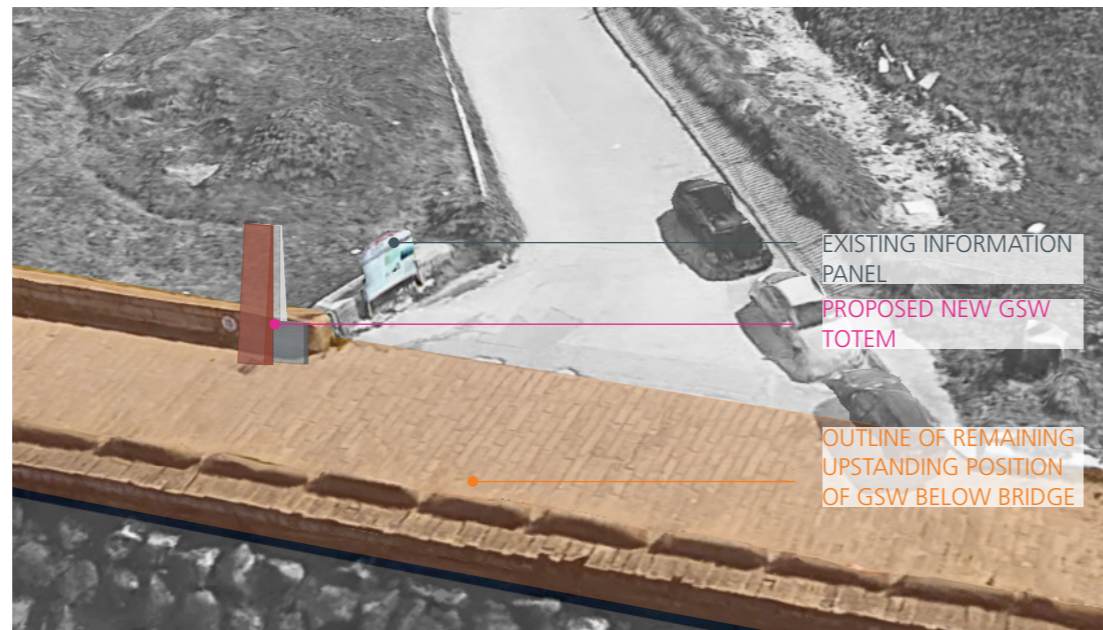


## Sections - Area 7 of Great South Wall: Existing South Wall to Lighthouse

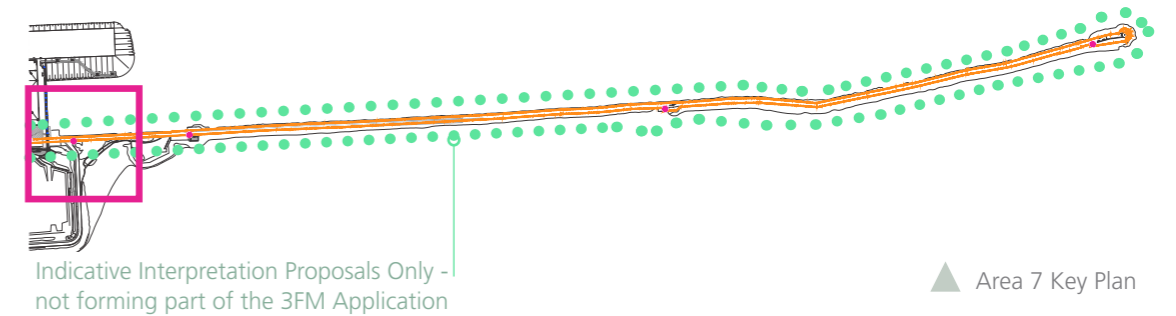
### Great South Wall - Information Panel

This area constitutes the beginning of the publicly accessible Pigeon House walk and also allows for vehicle access to the carpark area further eastwards along the wall. There is an existing information panel at this location but it is outdated and would greatly benefit from being replaced with a new interpretive totem featuring an integrated information panel. This update will ensure consistency with the rest of the interpretation package, enhancing the overall visitor experience and providing a more engaging and informative exploration of the Great South Wall.

As highlighted in the key plan, the interpretation proposals for this area are indicative only and are not included in the 3FM application. Given the highly sensitive nature of the existing GSW in this location, any proposals to place vertical markers on the current surface will require additional conservation considerations. Therefore, we recommend addressing these proposals through a separate planning consent in the future to ensure appropriate oversight and preservation.



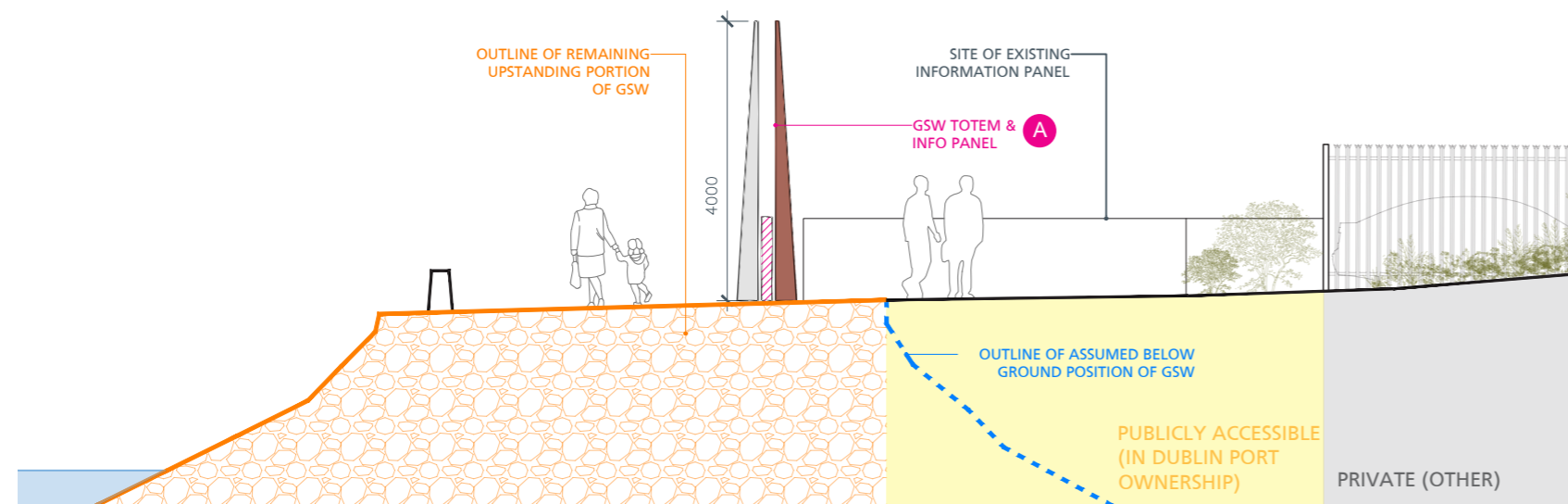
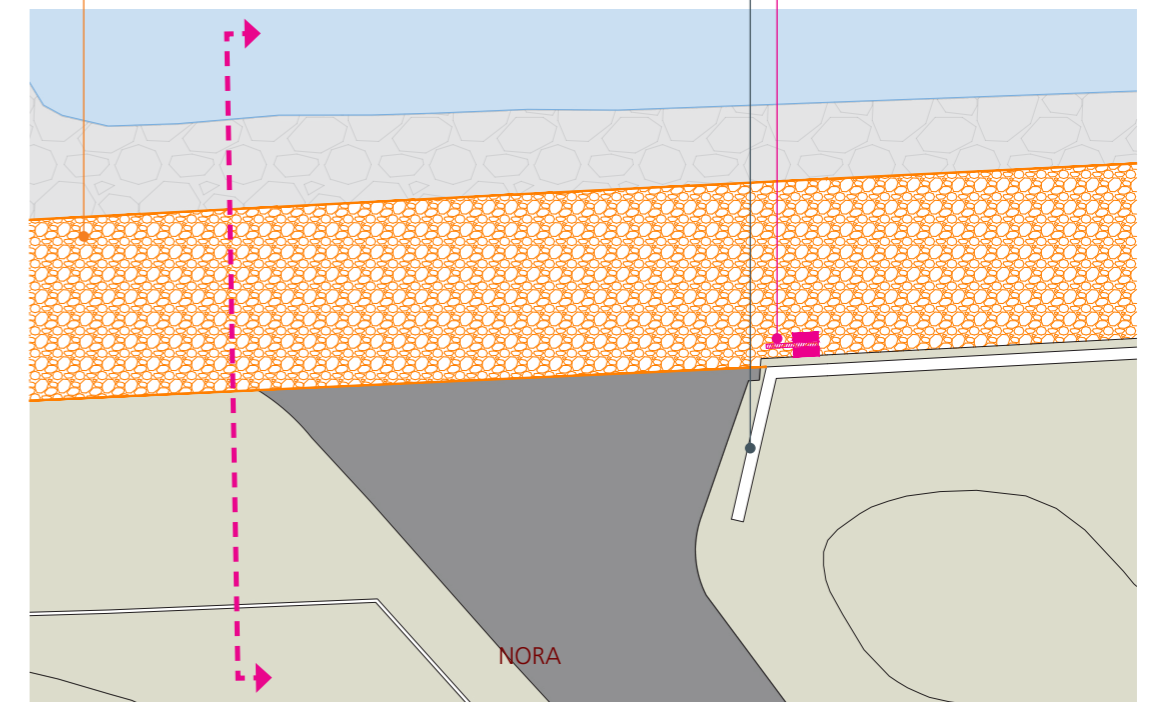
▲ Aerial View of existing conditions with GSW highlighted



PUBLICLY ACCESSIBLE PORTION OF THE GREAT SOUTH WALL (POOLBEG LIGHTHOUSE WALKING ROUTE)

▲ GSW TOTEM INTEGRATED WITH INFORMATION PANEL

EXISTING INFORMATION PANEL TO BE REPLACED



▲ Section 17-17  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



### Section 03



## Sections - Area 7 of Great South Wall: Existing South Wall to Lighthouse

### Great South Wall - Car Park

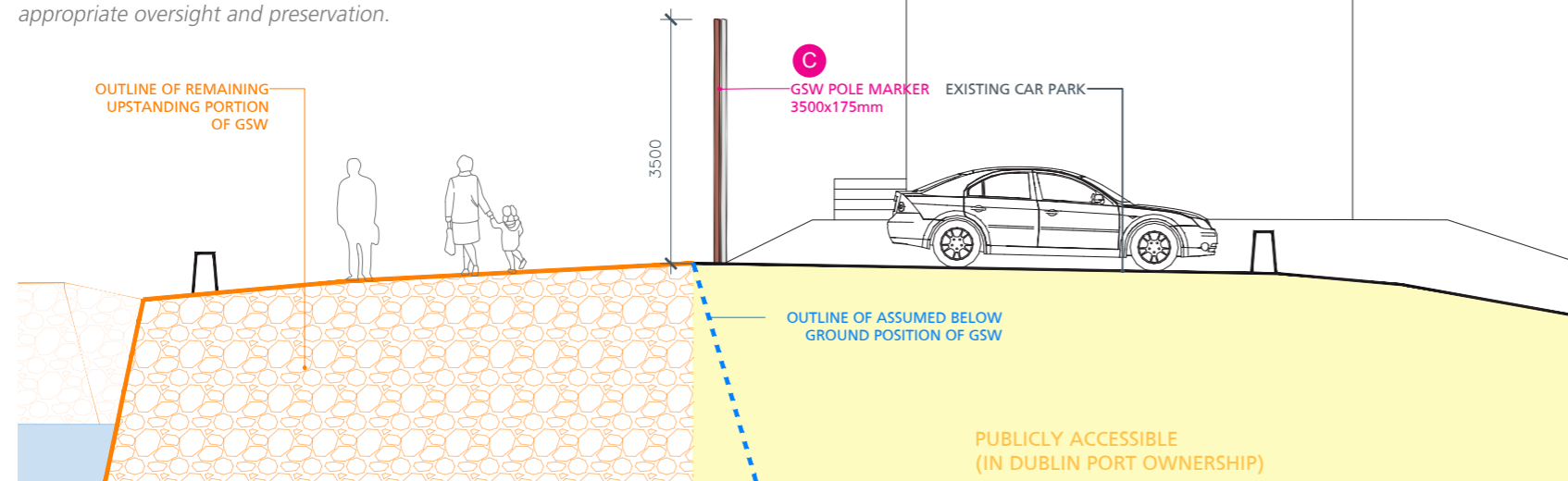
This parking area marks the conclusion of vehicular access on the Great South Wall, beyond which the wall is accessible solely as a public walking route. It serves as the primary entry point to the Great South Wall for pedestrians, who typically traverse the adjacent sand dunes to reach this spot.

We propose installing a vertical pole marker at this location. Aligned with the overall narrative, this marker will provide information about distance and its position relative to the entire length of the wall. This initiative aims to help visitors orient themselves and develop a deeper appreciation for the true extent of the wall, beyond the familiar Pigeon House walk section.



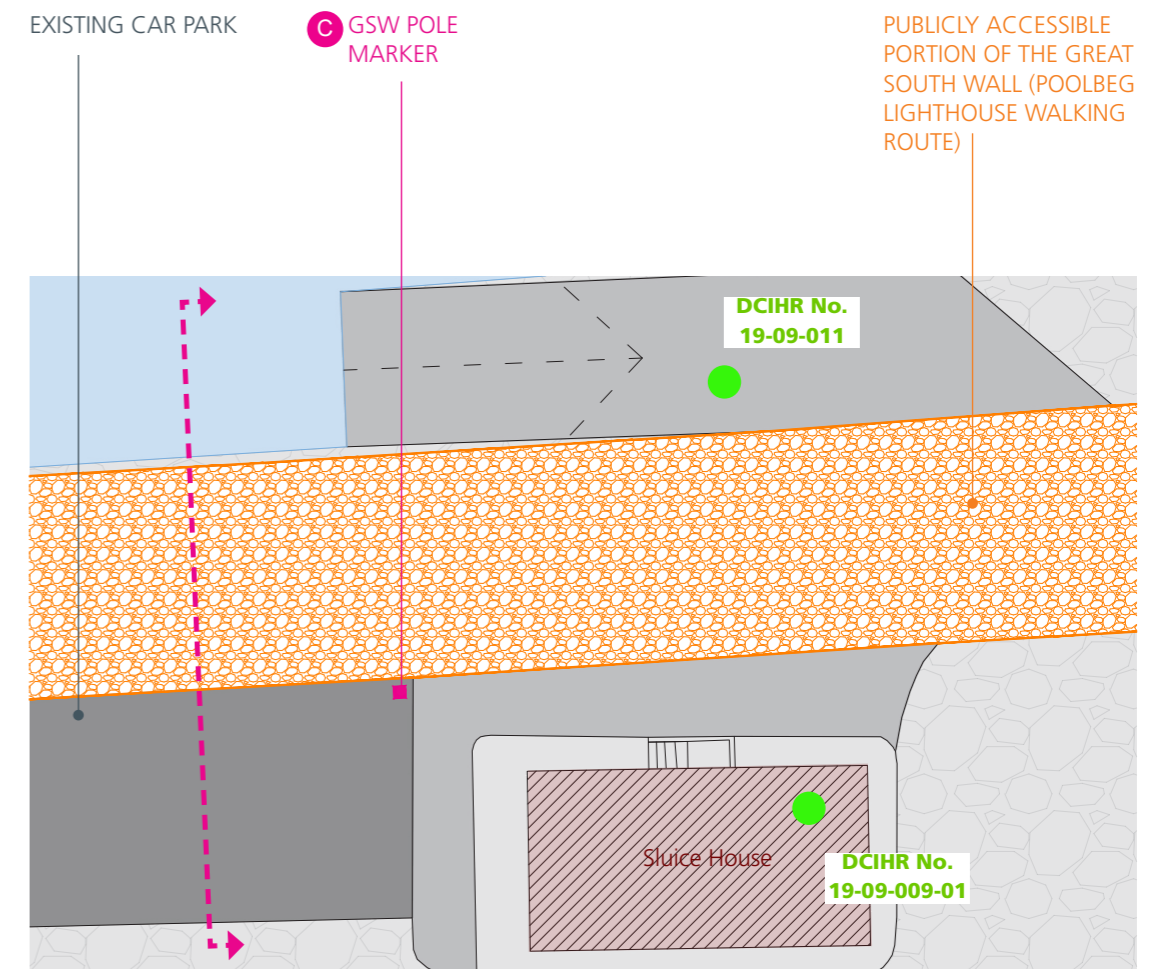
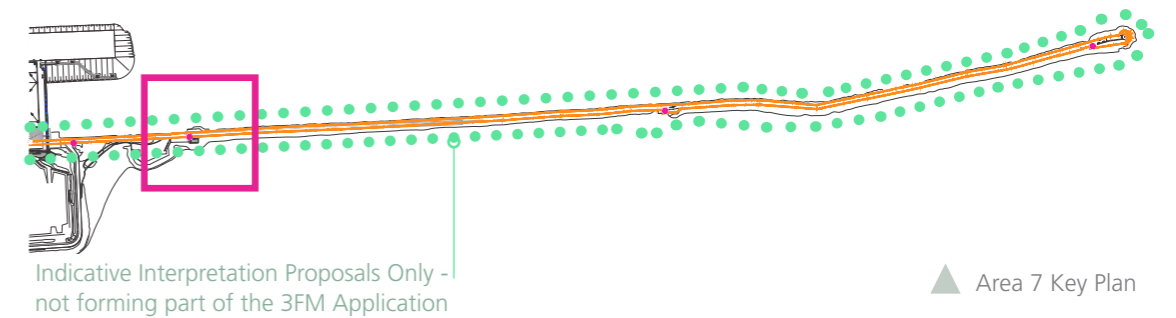
▲ Street View of existing Car Park conditions on the GSW

As highlighted in the key plan, the interpretation proposals for this area are indicative only and are not included in the 3FM application. Given the highly sensitive nature of the existing GSW in this location, any proposals to place vertical markers on the current surface will require additional conservation considerations. Therefore, we recommend addressing these proposals through a separate planning consent in the future to ensure appropriate oversight and preservation.



▲ Section 18-18  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



### Section 03



## Sections - Area 7 of Great South Wall: Existing South Wall to Lighthouse

### Great South Wall - Midpoint

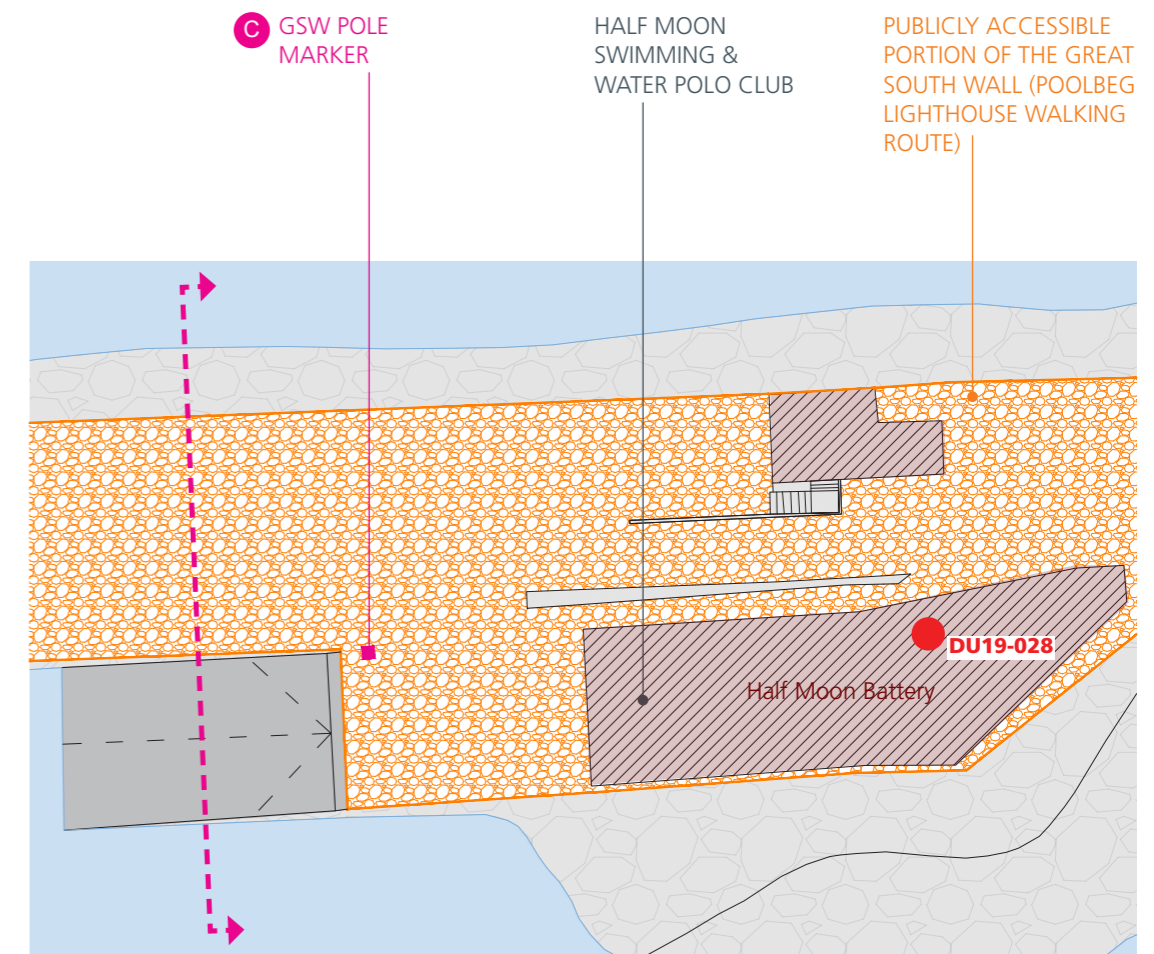
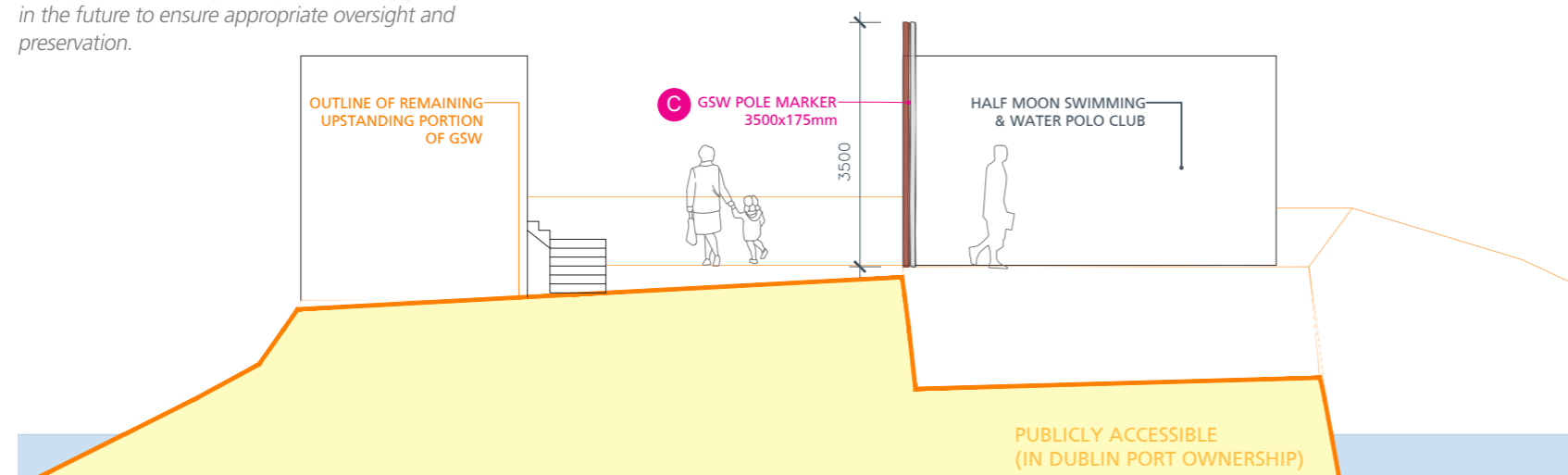
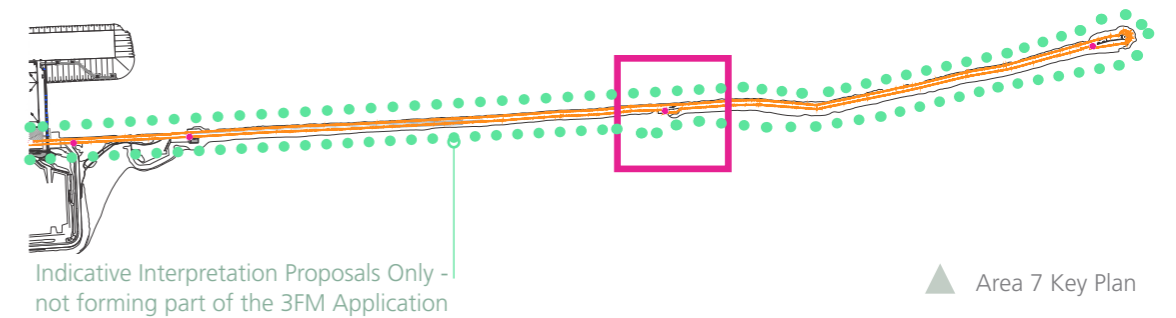
This area serves as the midpoint along the Great South Wall walking route to Poolbeg Lighthouse. A strategically placed interpretation marker here could not only indicate the distance covered along the GSW but also invite visitors to pause and appreciate the historical context of the surrounding buildings.

Similar to other sections, where interpretive markers enhance the visitor experience, this marker will encourage a natural rest point for visitors. It will provide insights into the significance of the Great South Wall, guiding them to understand its historical importance beyond the immediate surroundings. This approach ensures that every step along the wall becomes an educational journey, enriching visitors' understanding and appreciation of this iconic structure.

As highlighted in the key plan, the interpretation proposals for this area are indicative only and are not included in the 3FM application. Given the highly sensitive nature of the existing GSW in this location, any proposals to place vertical markers on the current surface will require additional conservation considerations. Therefore, we recommend addressing these proposals through a separate planning consent in the future to ensure appropriate oversight and preservation.



▲ Street View of existing Half Moon Swimming & Water Polo Club



▲ Section 19-19  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



### Section 03



## Sections - Area 7 of Great South Wall: Existing South Wall to Lighthouse

### Great South Wall - Poolbeg Lighthouse

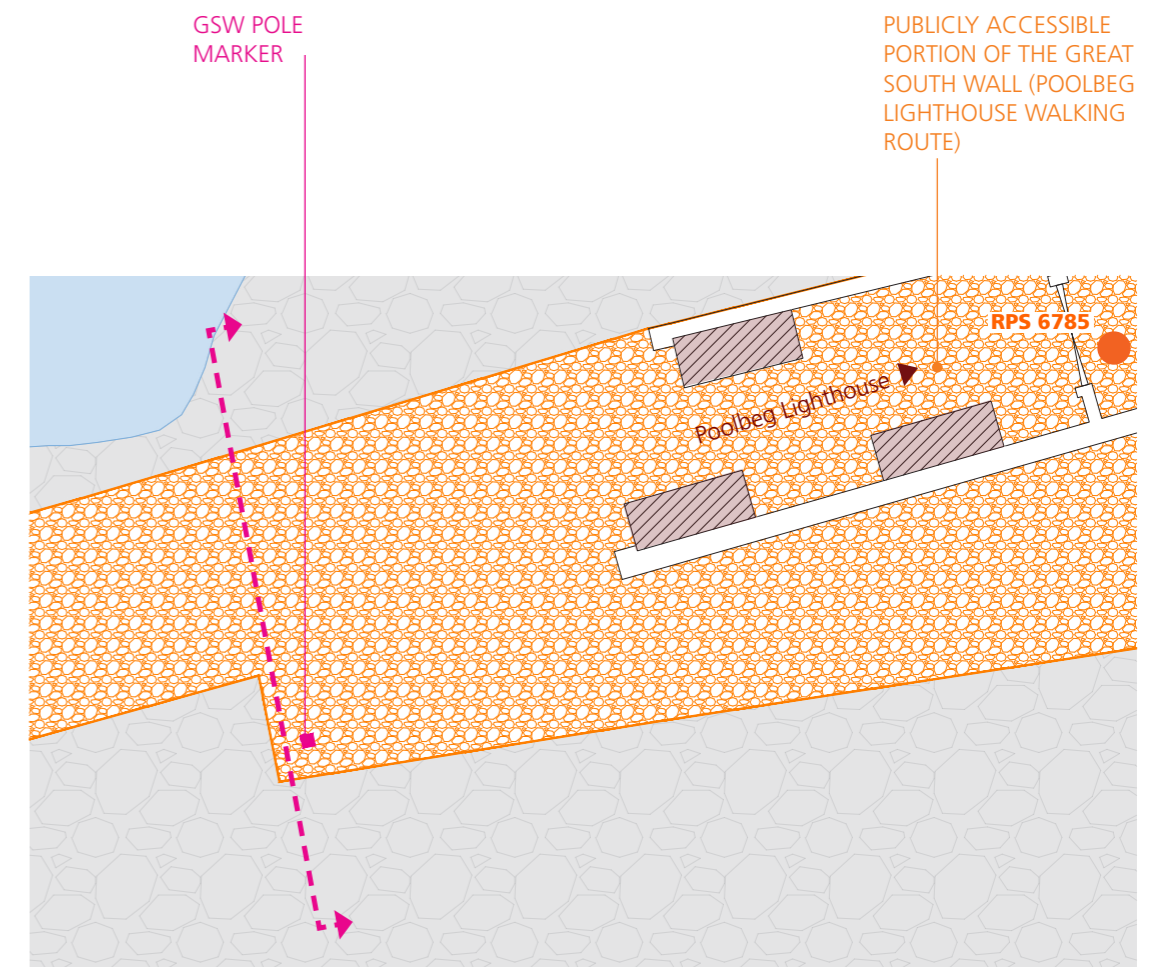
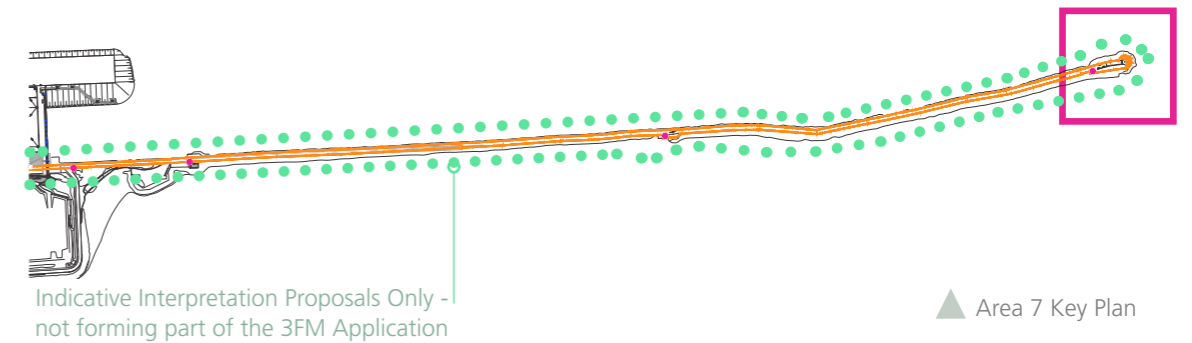
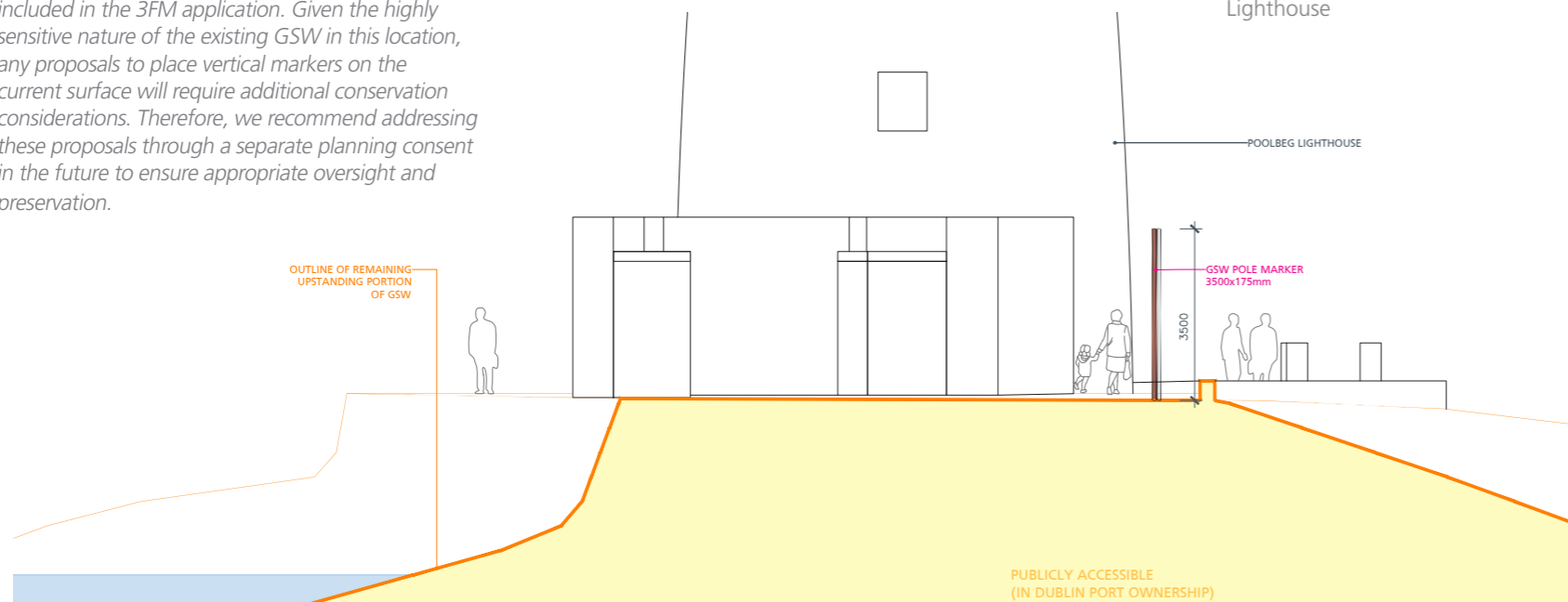
This area at the Poolbeg Lighthouse signifies the culmination of the Great South Wall. An interpretive marker positioned here could not only highlight the distance traveled along the GSW but also provide a fitting conclusion to the walk, harmonizing with the presence of the historic Poolbeg Lighthouse.

As visitors reach this final point, the marker will serve as a poignant reminder of the journey taken along the Great South Wall, offering insights into its significance and historical context. This thoughtful addition aims to leave a lasting impression, ensuring that visitors depart with a deeper appreciation for this remarkable landmark and its enduring importance.

As highlighted in the key plan, the interpretation proposals for this area are indicative only and are not included in the 3FM application. Given the highly sensitive nature of the existing GSW in this location, any proposals to place vertical markers on the current surface will require additional conservation considerations. Therefore, we recommend addressing these proposals through a separate planning consent in the future to ensure appropriate oversight and preservation.



▲ Street View towards Poolbeg Lighthouse



▲ Section 20-20  
1:100

- GSW Visible Above Ground
- GSW Position Below Ground
- - - Section Line Through Site
- Proposed Removal of GSW

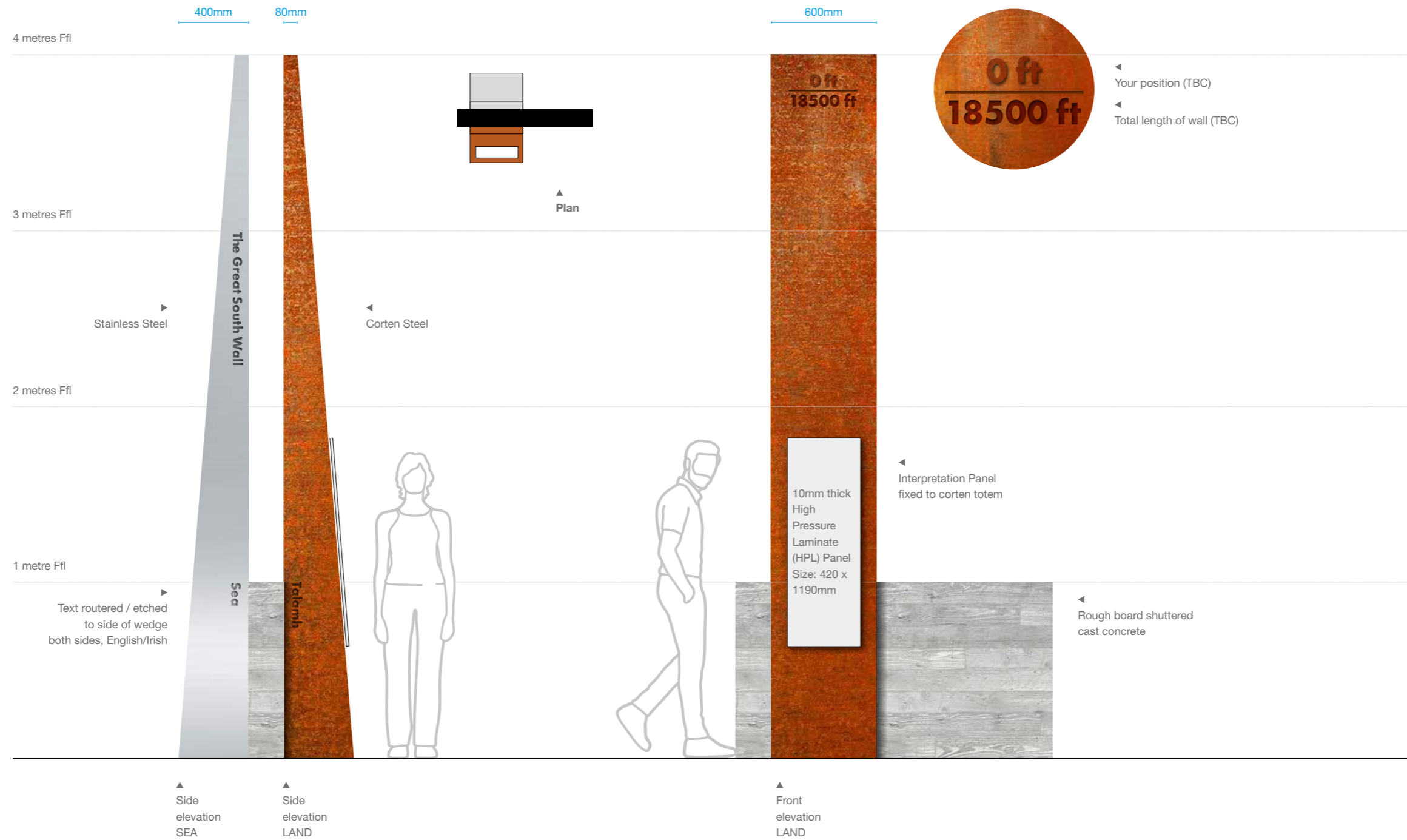
- GSW Totem / Pole Markings
- GSW Ground Surface Markings
- Proposed Location of GSW Primary Totems
- Proposed Location of GSW Intermediate Markers



### Section 03



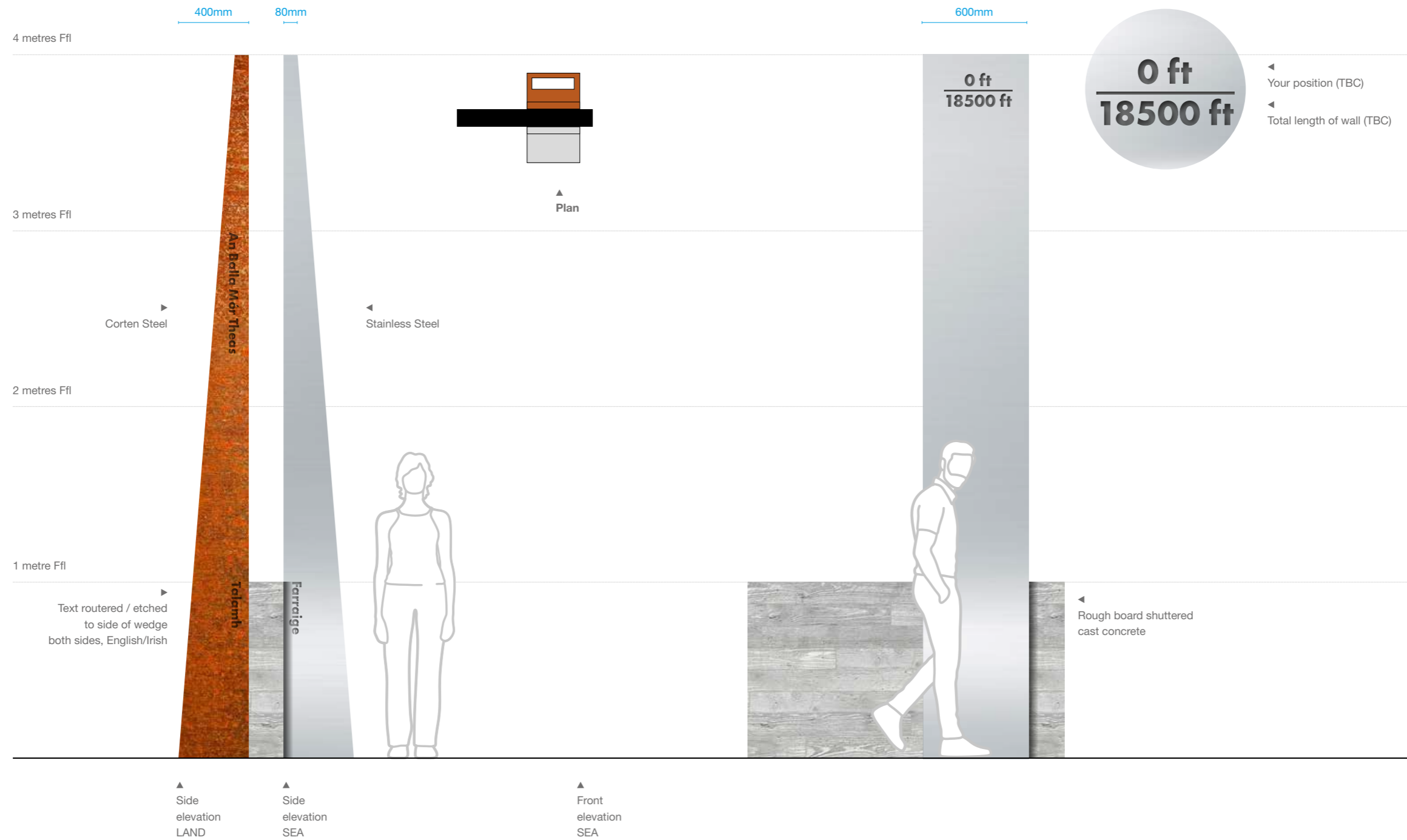
Section 04 - Proposed Interpretation Elements: Prepared by "Bright"  
Proposed Interpretation Elements: Type A



▲ Interpretation Element GSW Totem Type A  
Courtesy of BRIGHT



Proposed Interpretation Elements: Type A



▲ Interpretation Element GSW Totem Type A  
Courtesy of BRIGHT



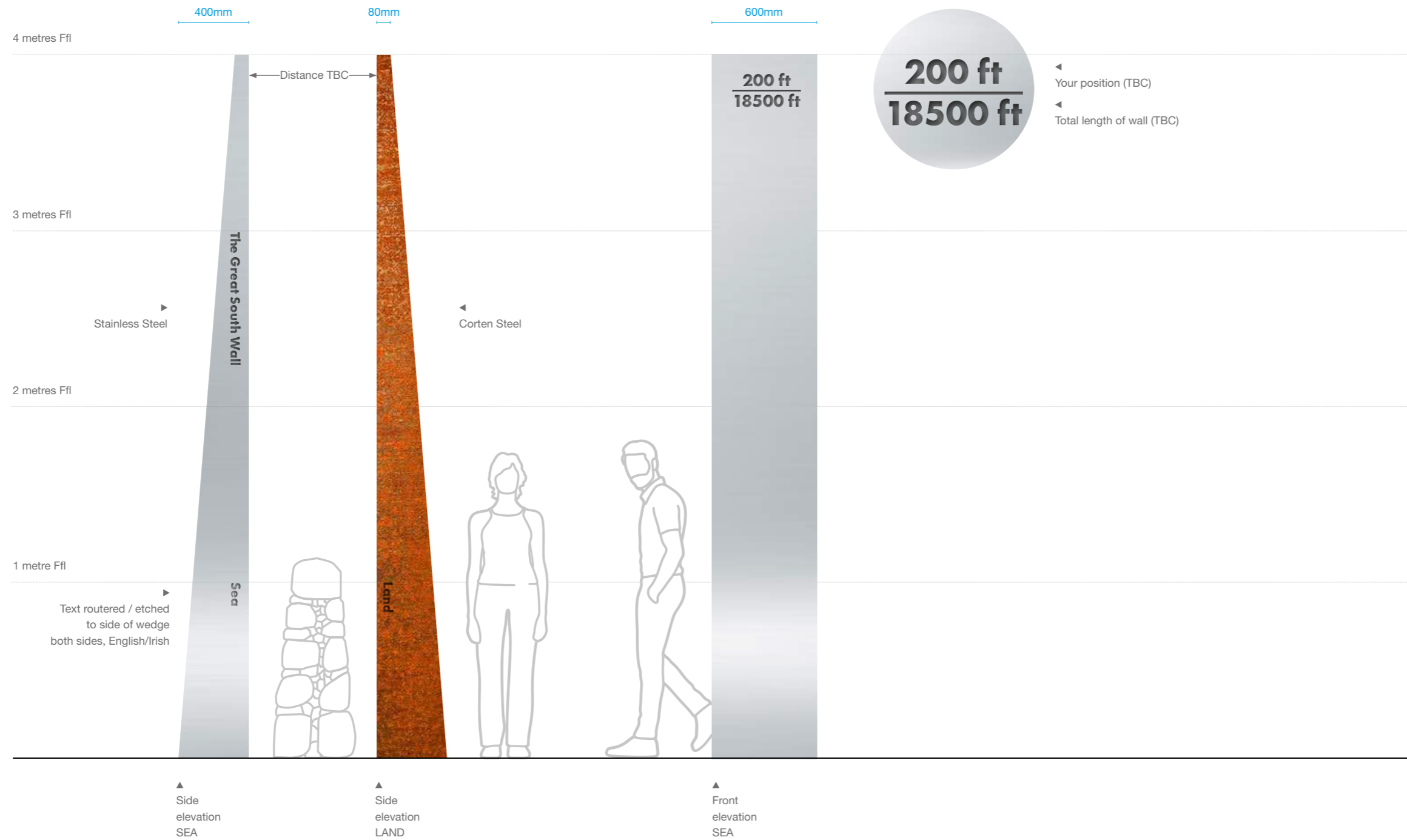
Proposed Interpretation Elements: Type B



▲ Interpretation Element GSW Totem Type B  
Courtesy of BRIGHT



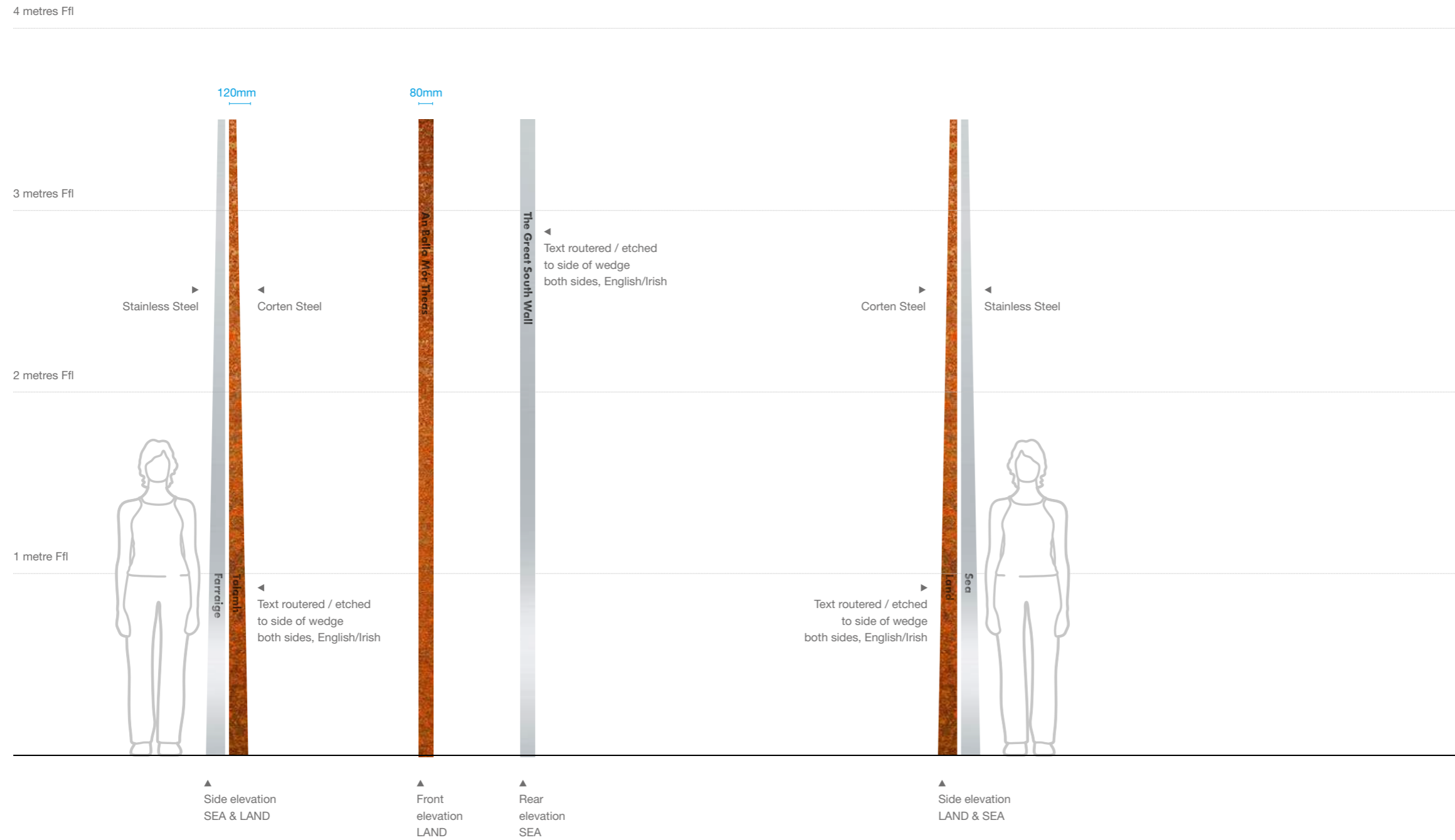
Proposed Interpretation Elements: Type B



▲ Interpretation Element GSW Totem Type B  
Courtesy of BRIGHT



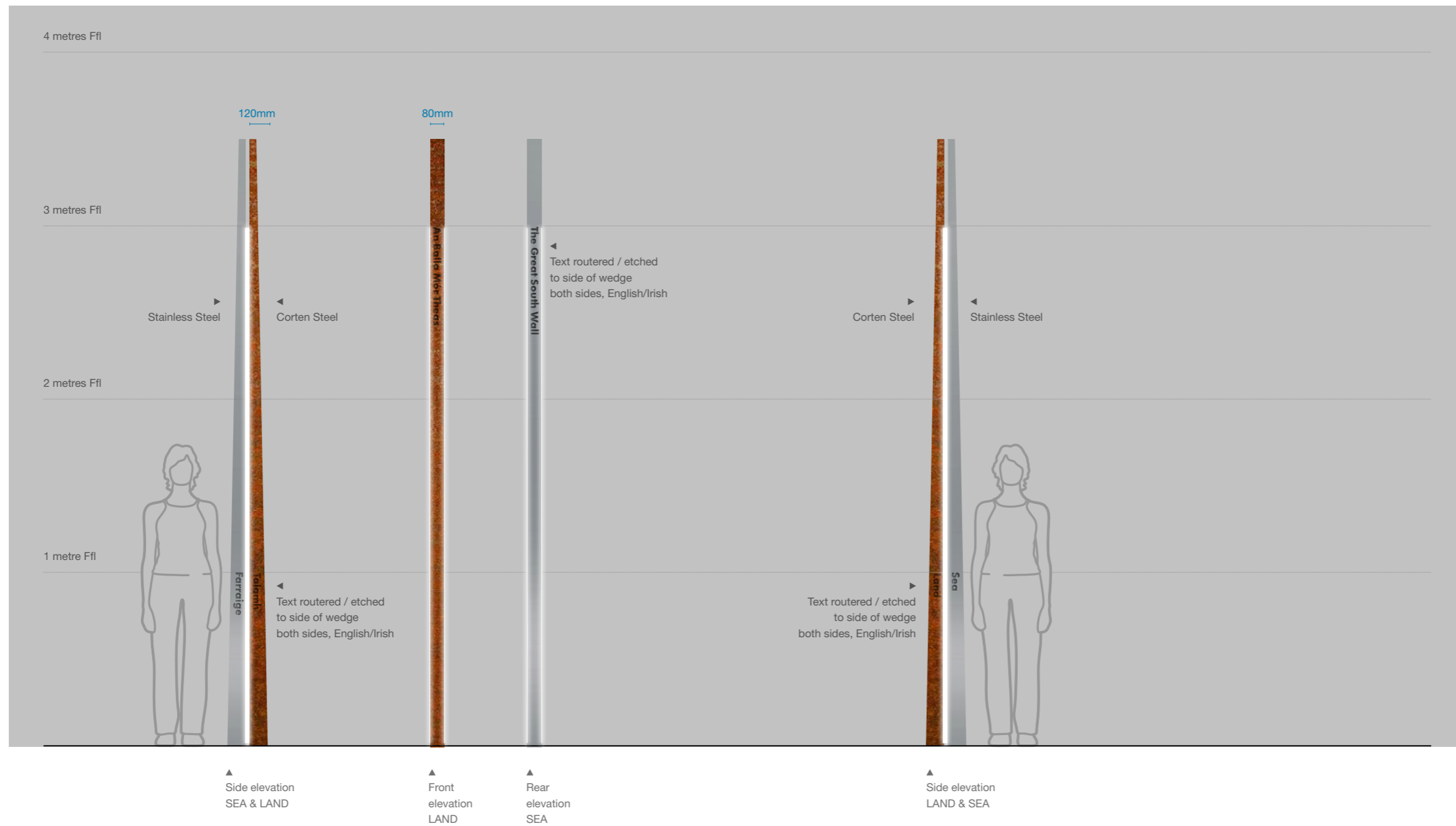
Proposed Interpretation Elements: Type C



▲ Interpretation Element GSW Totem Type C  
Courtesy of BRIGHT



Proposed Interpretation Elements: Type D



▲ Interpretation Element GSW Totem Type D  
Courtesy of BRIGHT



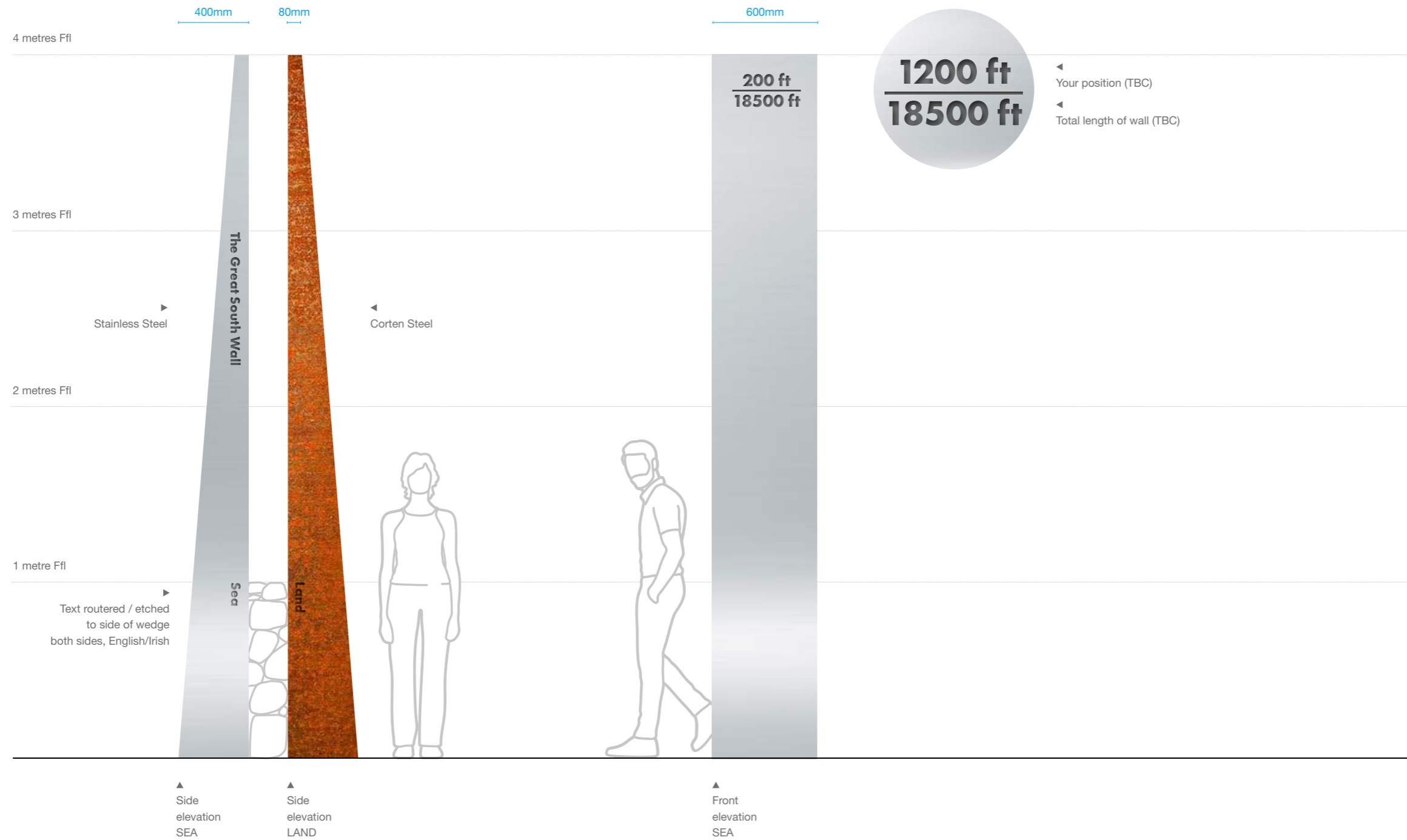
Proposed Interpretation Elements: Type E



▲ Interpretation Element GSW Totem Type E  
Courtesy of BRIGHT



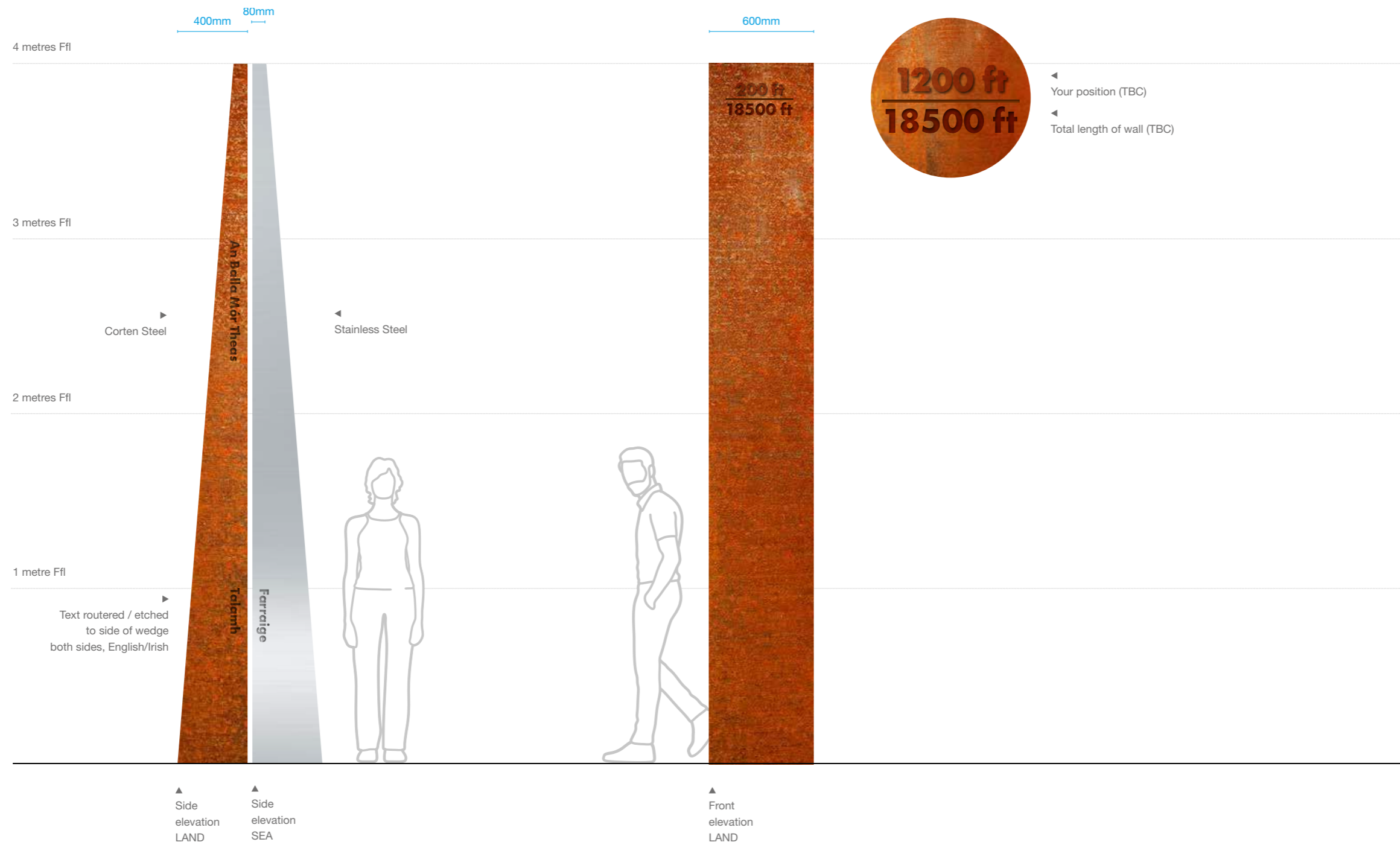
Proposed Interpretation Elements: Type E



▲ Interpretation Element GSW Totem Type E  
Courtesy of BRIGHT



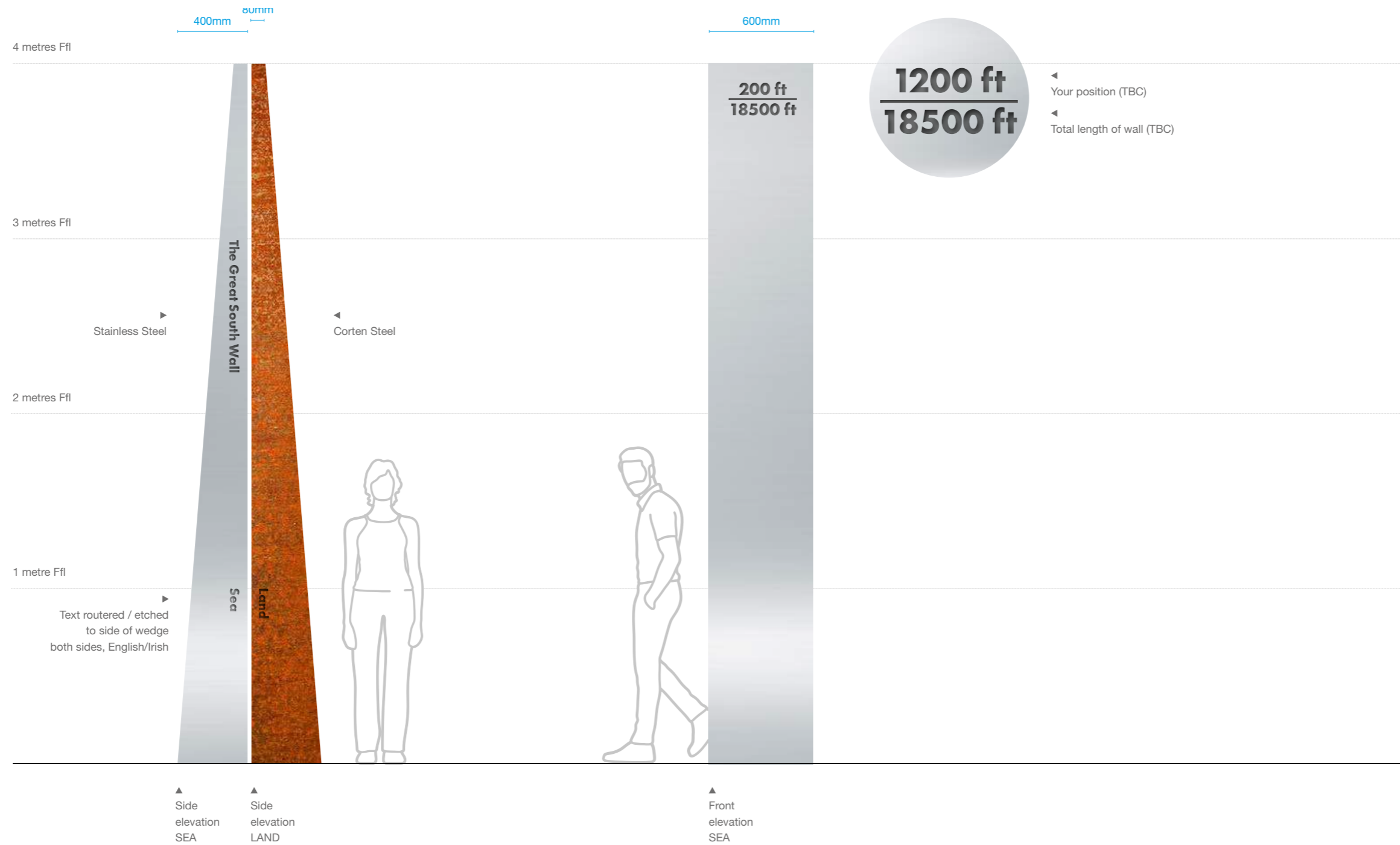
Proposed Interpretation Elements: Type F



▲ Interpretation Element GSW Totem Type F  
Courtesy of BRIGHT



Proposed Interpretation Elements: Type F



▲ Interpretation Element GSW Totem Type F  
Courtesy of BRIGHT



Proposed Interpretation Elements: Type **G**

Type **G**

Aluminium Double sided Noise reduction barrier with applied external grade vinyl with graphic reproduction of historic image of Great Sea Wall on both sides.

4 metres Ffl

Distance TBC

3 metres Ffl

2 metres Ffl

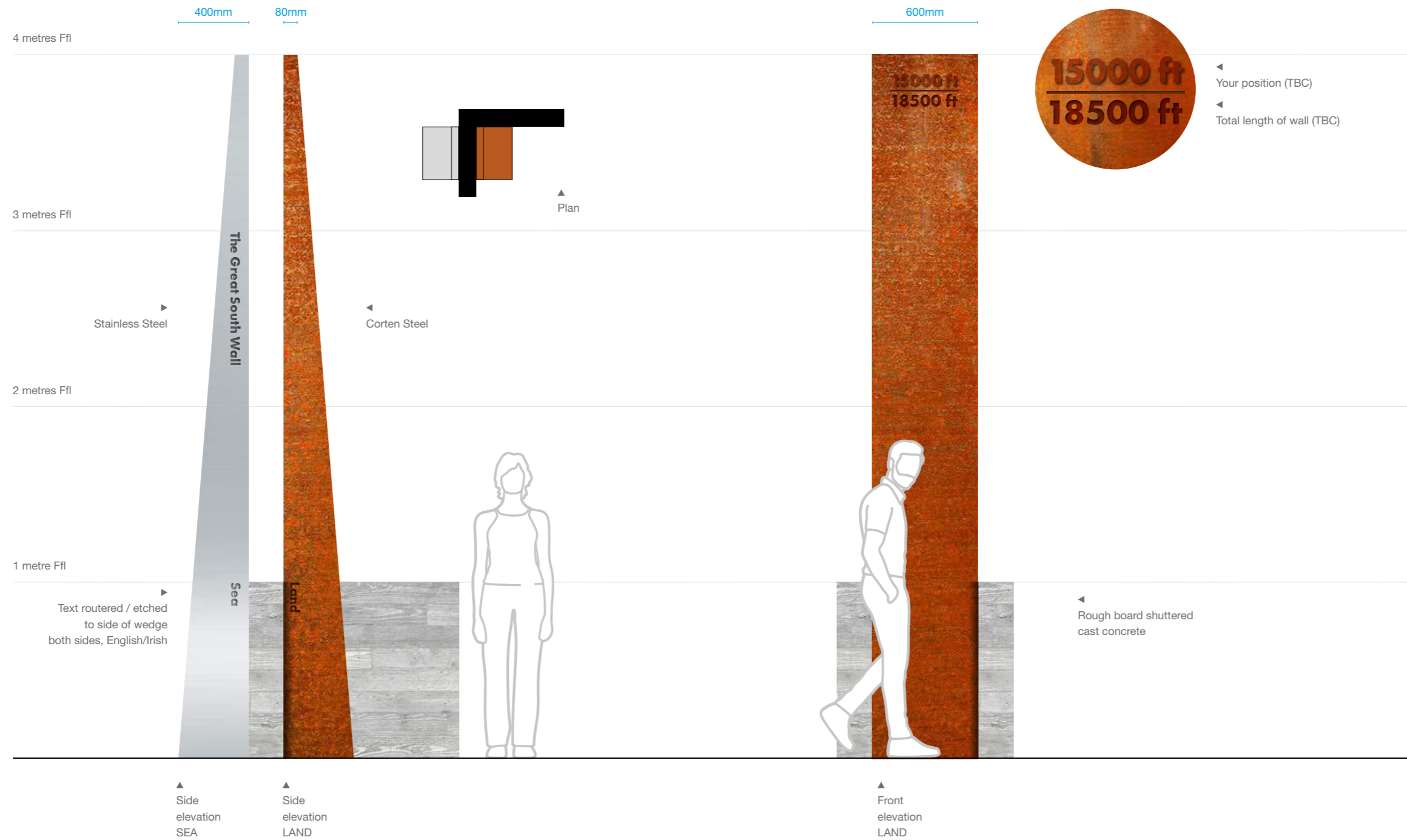
1 metre Ffl



◀ View of the Pigeon House and South Wall, Dublin, Ireland. Painted by William Sadler, 1810. National Gallery of Ireland



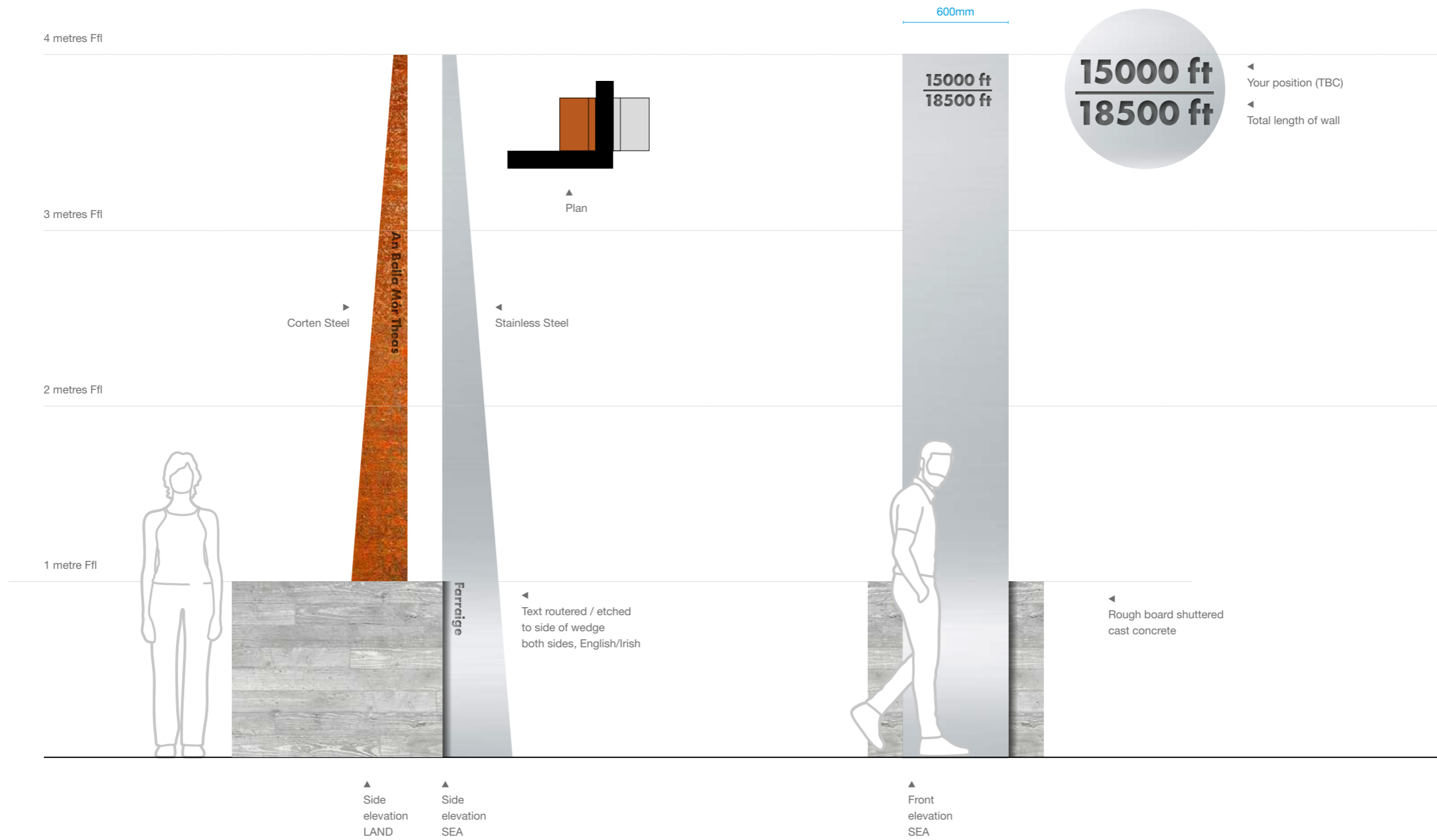
Proposed Interpretation Elements: Type H1



▲ Interpretation Element GSW Totem Type H1  
Courtesy of BRIGHT



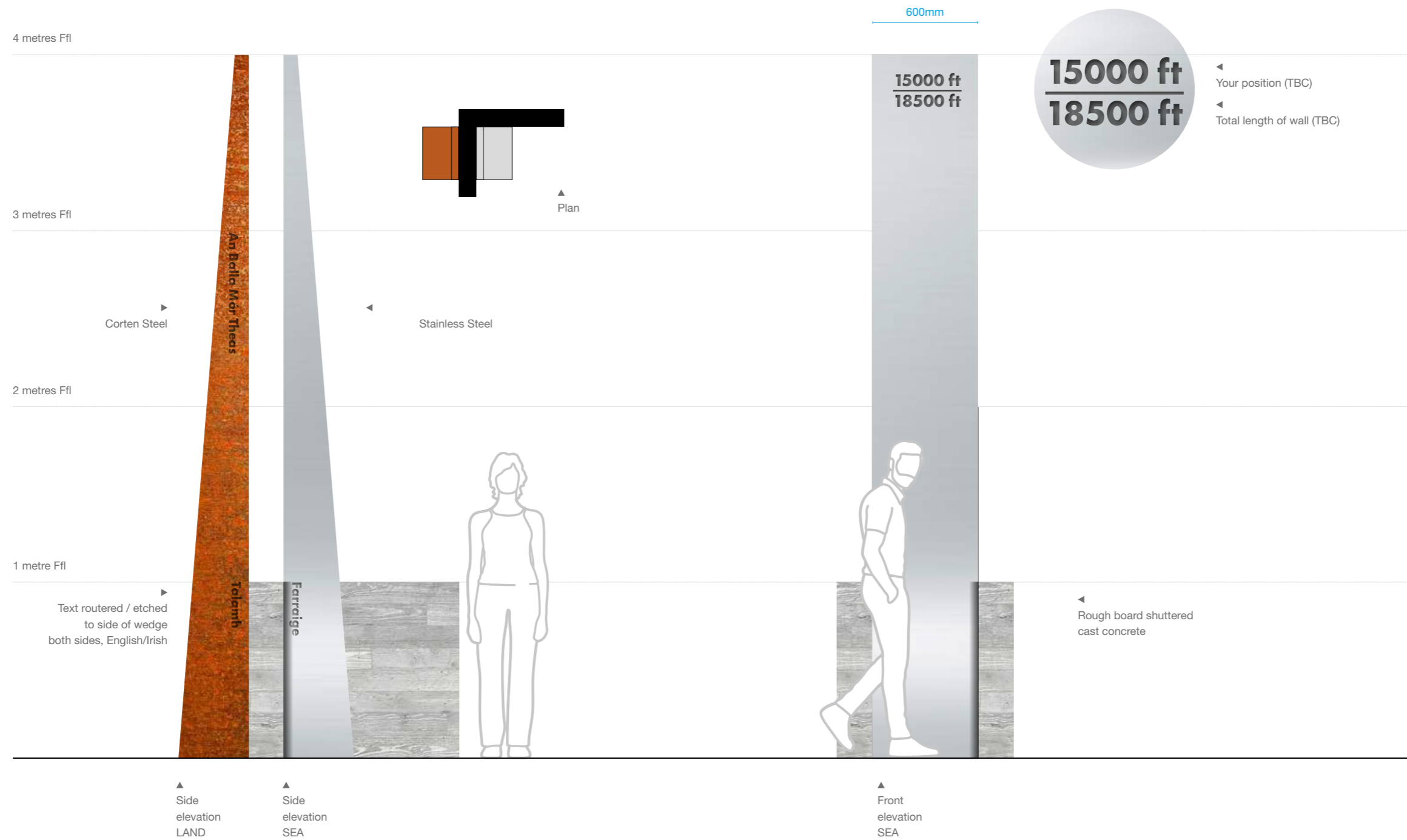
Proposed Interpretation Elements: Type **H1**



▲ Interpretation Element GSW Totem Type H1  
Courtesy of BRIGHT



Proposed Interpretation Elements: Type H2



▲ Interpretation Element GSW Totem Type H2  
Courtesy of BRIGHT



Proposed Interpretation Elements: Type H2

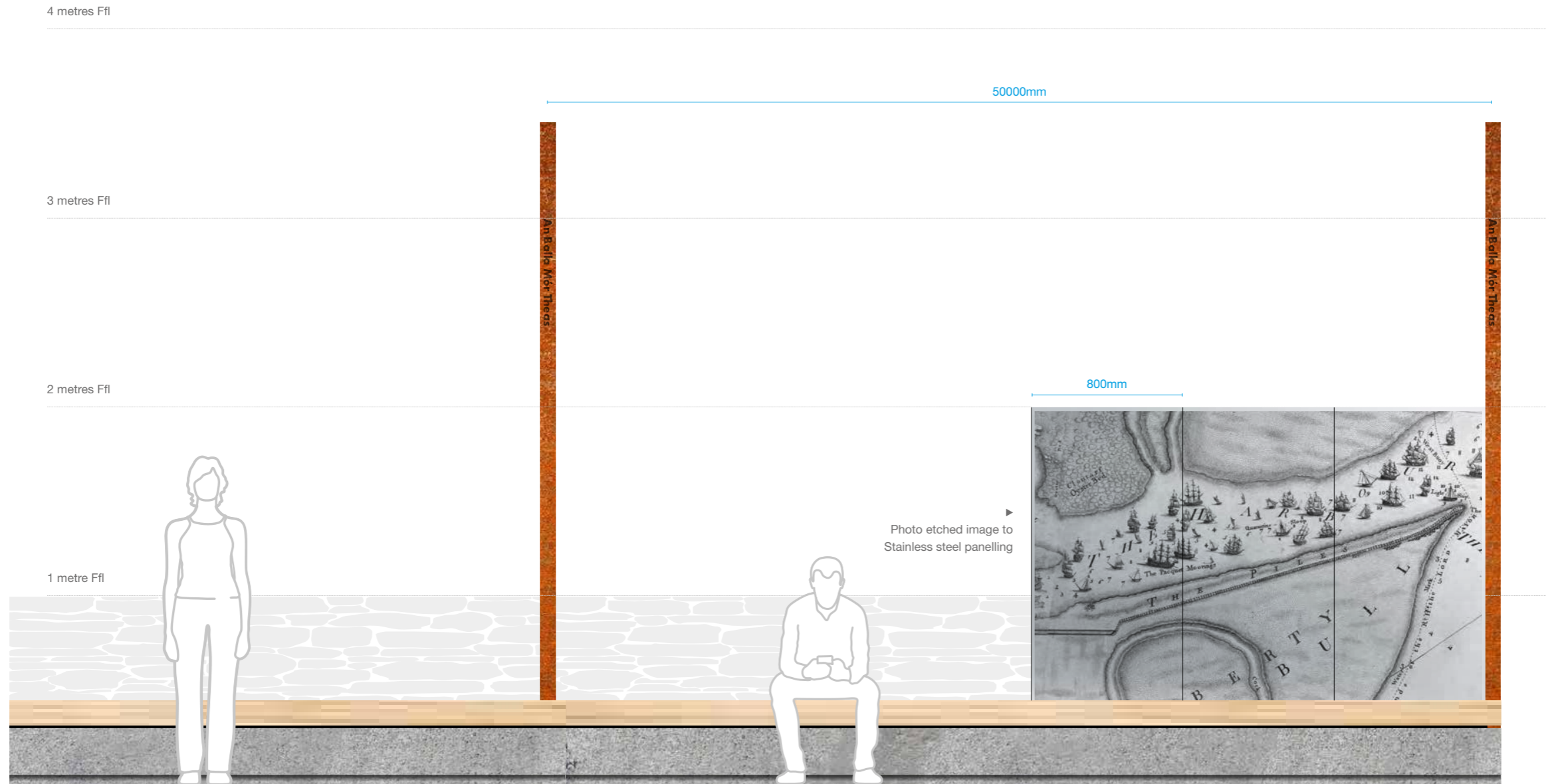


▲ Interpretation Element GSW Totem Type H2  
Courtesy of BRIGHT

Section 04



Proposed Interpretation Elements: Type J

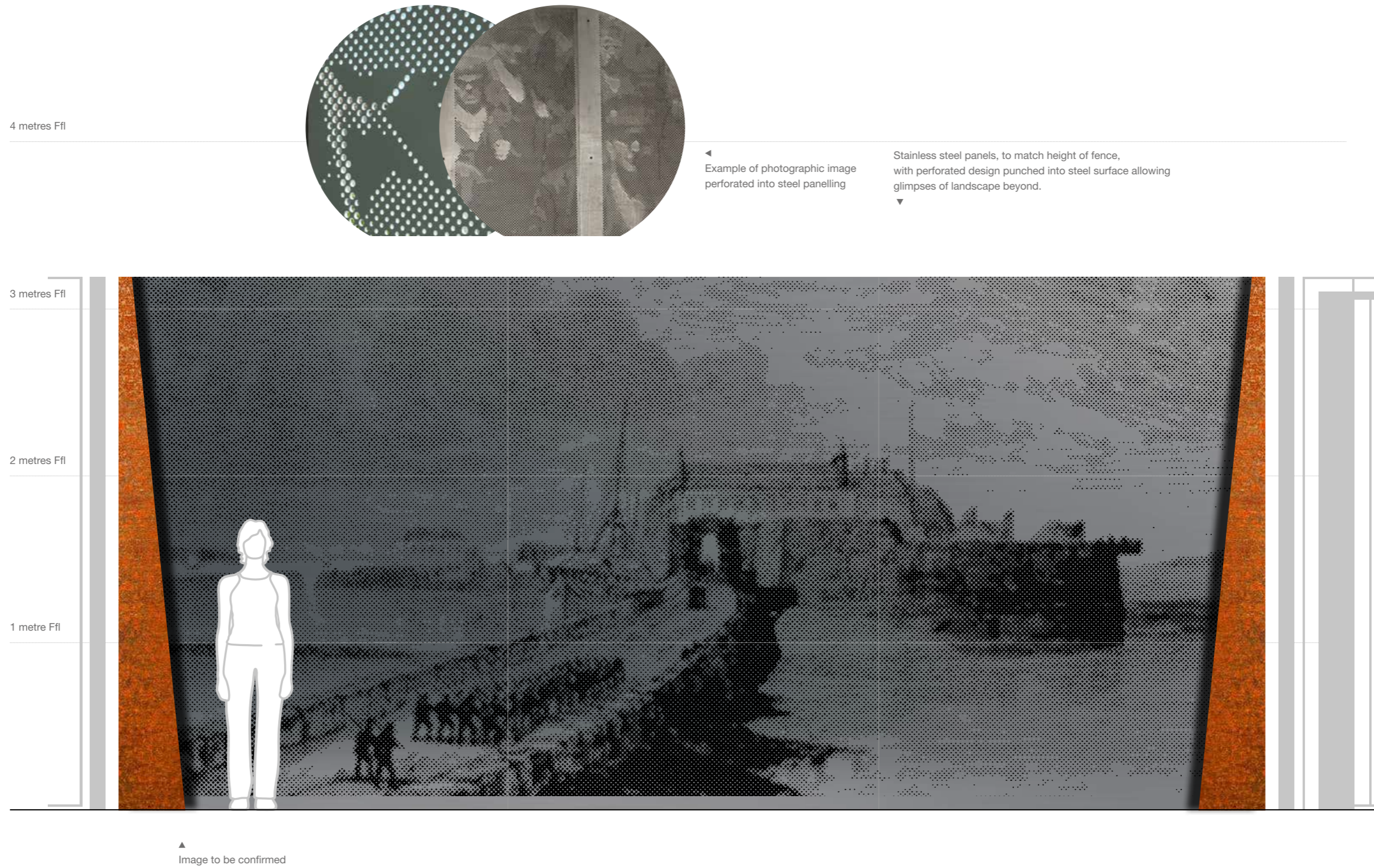


▲ Interpretation Element GSW Bench & Graphic Panel Type J  
Courtesy of BRIGHT

Section 04



Proposed Interpretation Elements: Type **K**



▲ Interpretation Element Graphic Panel Type K  
Courtesy of BRIGHT



Conclusion



**In conclusion, the proposed initiatives outlined in this plan are aimed at restoring the legibility and historical significance of the Great South Wall, which has been obscured over time by various developments in the area.**

By strategically deploying vertical markers, special totems, and interpretive elements, the project aims to reestablish the legibility of the wall's original route, which has been obscured over time. These measures not only serve to educate the public about the wall's historical importance but also to enhance the overall visitor experience by providing clear guidance along the historic route. Additionally, the incorporation of mitigation measures to address disturbances caused by the 3FM Project demonstrates a commitment to preserving the wall's integrity amidst ongoing urban transformations.

Furthermore, the design's focus on integrating historical elements seamlessly into the landscape and infrastructure ensures a harmonious blend of the old and the new. By utilising salvaged stone for wall reconstruction/restoration and incorporating ground surface markings, the project not only respects the historical context but also creates a visually consistent experience that enhances the area's cultural narrative. These efforts are expected to have a positive impact on public realms such as the maritime village, fostering a sense of connection to the area's past and creating vibrant, engaging environments that celebrate its heritage. Overall, the project represents a balanced approach to urban development, one that honors the past while embracing the future. The intention of this overall interpretation package is to introduce indicative areas to be further developed in collaboration with DCC and other stakeholders in the future.



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